

NOTES

ALL STRUCTURAL STEEL SHALL CONFORM TO THE LATEST
REVISION OF THE SPECIFICATIONS ASTM A36 UNLESS
OTHERWISE NOTED.

FIELD BOLTS SHALL BE ASTM A325 7/8" HEAVY HEXAGON
STRUCTURAL BOLTS WITH ONE HEAVY SEMI-FINISHED NUT
& ONE HARDENED WASHER.

FABRICATE & ERECT IN ACCORDANCE WITH MSHC STANDARD SPECIFICATIONS.

FIELD TO INSTALL 5/8"Ø ROUND HEAD CARRIAGE BOLTS IN
FLEMING BRACKET HOLES WITH HEAD ON EXTERIOR SIDE OF
STRINGER.

SHOP CONNECTIONS: LH-E7018 or LH-E7028

FIELD CONNECTIONS: 7" ϕ H.S. BOLTS & WELD

HOLES: 15" ϕ
16" ϕ

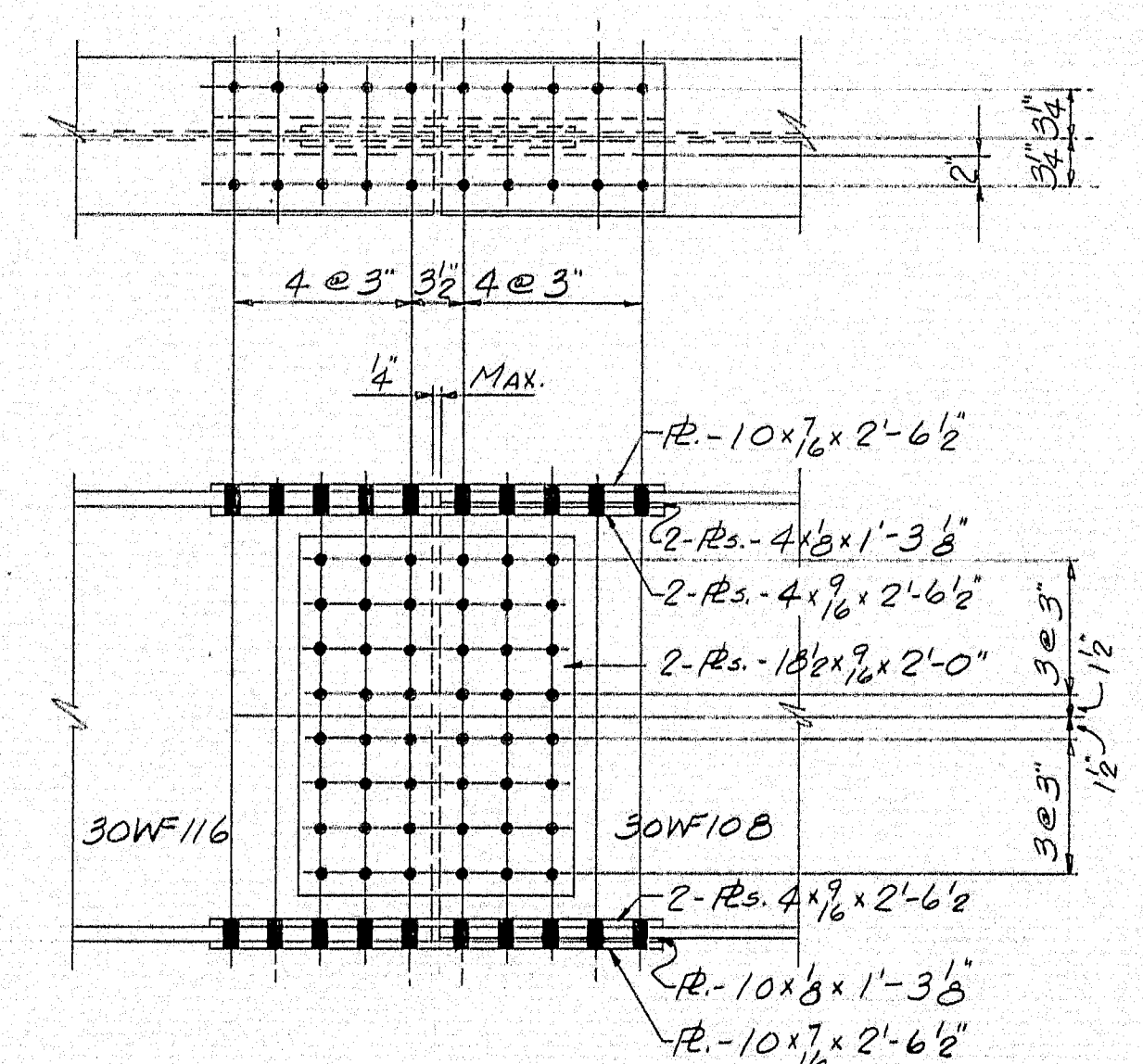
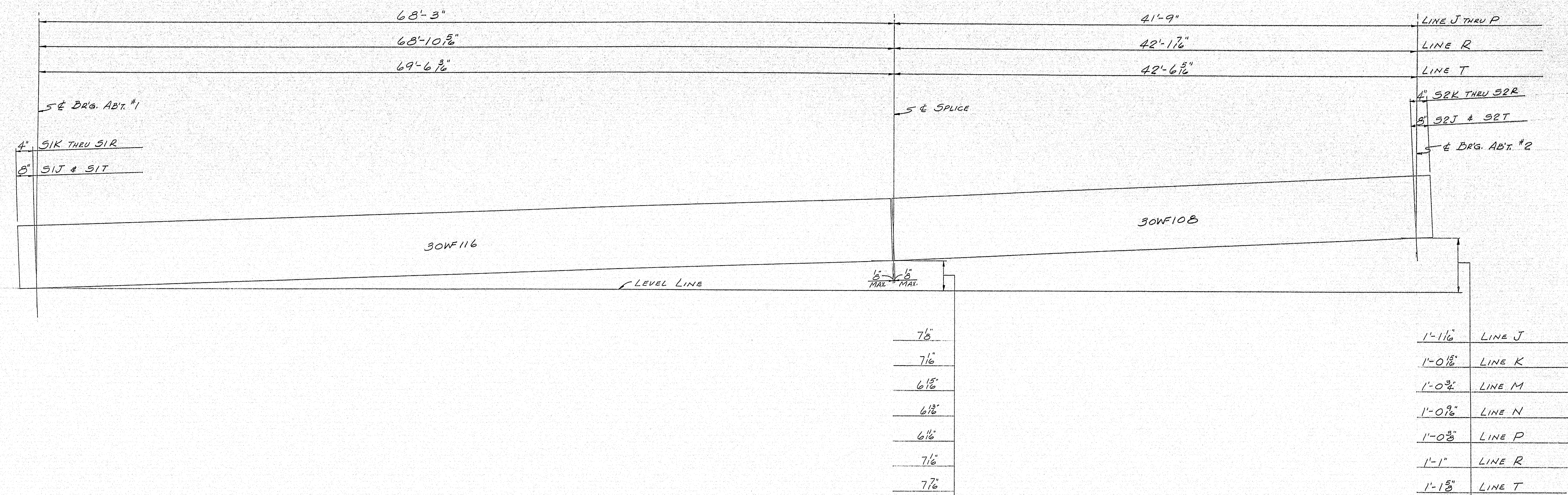
PAINT: STATE OF MAINE SPEC'S

APP'D. 8-25-65

PROJ. No. I-95-9(12)

FRAMING PLAN		NORTHBOUND	
PRINT ISSUE		<i>Bancroft & Martin Inc.</i> <i>Brewer, Maine</i> I-95 OVER E. BRANCH MATTAWAMKEAG RIVER OAKFIELD, MAINE	
2 SHOP	9-10-65		
5 S.H.C.	9-1-65		
3 CUST.	9-1-65		
2 F.A.	8-13-65	CUSTOMER <i>CIANCHETTE BRO'S, INC.</i> DESIGNER <i>M.S.H.C., BRIDGE DIVISION</i>	
2 F.A.	8-6-65		
DRAWN	7-22-65 R.A.M.		
REVISION	8-13-65 R.A.M.	ORDER <i>VERBAL</i> DWG. <i>B65-135-E2</i>	
REVISION			
REVISION			

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STRINGER SPLICE DETAIL

40-3/8" x 0-3/4" H.S. BOLTS FOR FLG. SPLICE
48-3/8" x 0-3" H.S. BOLTS FOR WEB SPLICE

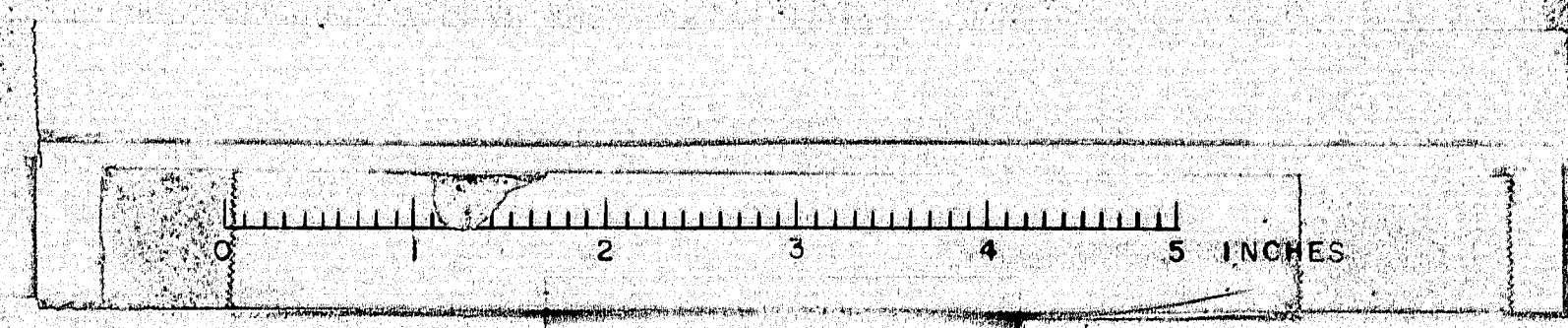
SHOP CONNECTIONS: LH-E7013 or LH-E7028
FIELD CONNECTIONS: 3/8" H.S. Bolt & WELD
HOLES: 1/2" &
PAINT: STATE OF MAINE SPEC'S.

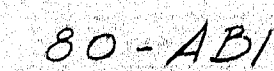
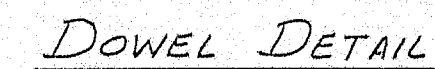
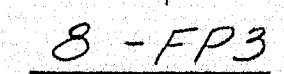
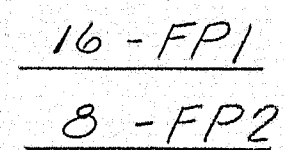
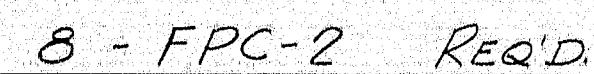
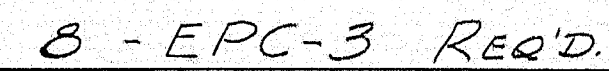
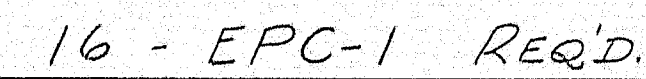
APP'D. AS NOTED 8-10-65 PROJ. No. I-95-9 (12)

STRINGER ELEVATION DIAGRAM & SPLICE DETAIL - NORTHBOUND

PRINT ISSUE			Bancroft & Martin Inc. Brewer, Maine	
NO.	DATE	BY		
5	S.H.C.	9-10-65	I-95 OVER E. BRANCH MATTAWAMKEAG RIVER OAKFIELD, MAINE	
3	CUST.	9-10-65		
2	SHOP	9-10-65		
2	F.A.	8-6-65	CUSTOMER CIANCHETTE BRO'S, INC. DESIGNER M.S.H.C., BRIDGE DIVISION	
DRAWN	8-4-65	R.A.M.		
REVISION				
REVISION				
REVISION			ORDER VERBAL	DWG. B65-135-E4

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SHIP		BILL OF MATERIAL				DWG. 665-135-51	
MARK	NO.	MARK	SHAPE	LENGTH	WT.	REMARKS	
EPC1	16		PEDESTAL				
	16	P3	R-7x2	1	0		A36
	16	P4	R-6x2	0	8		
	16	P5	R-5 1/2 x 2	0	11		
	16	P6	R-8 x 1 1/2	1	8		
	64	P7	R-2 x 1	0	34		
	32	P8	R-1 3/4 x 3/8	0	4		
EPC3	8		PEDESTAL				
	8	P9	R-8 x 2	1	0		A36
	8	P10	R-9 1/2 x 2	0	11		
	8	P11	R-7 x 2	0	8		
	8	P12	R-9 x 1 3/4	1	8		
	32	P13	R-2 1/2 x 1	0	74		
	16	P8	R-1 3/4 x 3/8	0	4		
FPC2	8		PEDESTAL				
	8	P14	R-7 1/2 x 2	1	2		A36
	8	P11	R-7 x 2	0	8		
	8	P15	R-10 x 1 1/2	1	8		
	32	P16	R-3 1/2 x 1	0	5 1/2		
	16	P8	R-1 3/4 x 3/8	0	4		
	48	R1	ROD-1" φ	0	1/2		A36
FP1	16		8 x 8	1	8	FABCO PAD S447	
FP2	8		9 x 8	1	8	D _o	D _o
FP3	8		10 x 8	1	8	D _o	D _o
AB1	80		ROD-1" φ	1	3	THREADED & SWEDGED	A36
	160		1" φ HEX NUT			REQ. # 3880	
	80		1" φ STD. WASH ^{ER}				

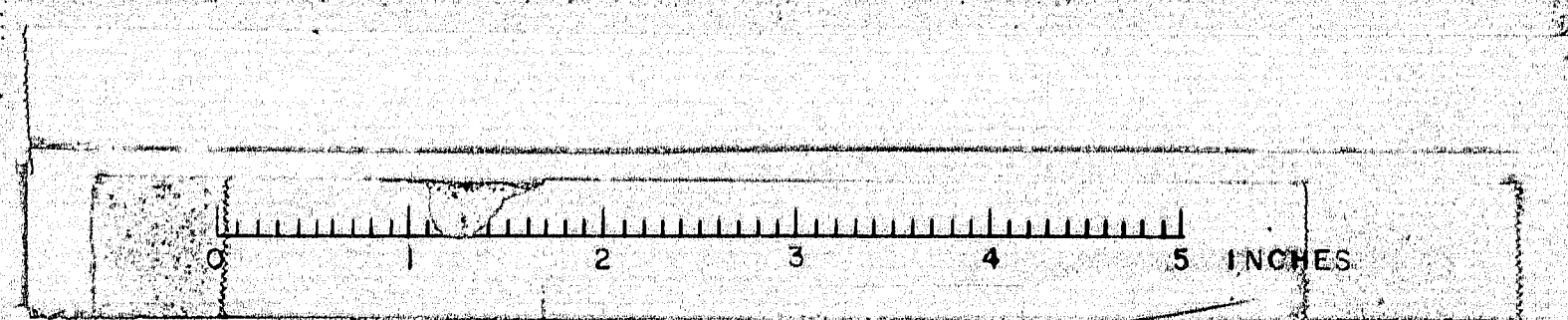
WELD WITH LH-E7028 OR
LH-E6028 or SAW-1 & PREHEAT
SHOP CONNECTIONS: 1" TO 2" THK. MAT. TO 50" F.
FIELD CONNECTIONS: BOLT & WELD
HOLES: 1 3/8" & U.N.
PAINT: STATE OF MAINE SPEC'S & SEE
PAINT NOTE THIS SHEET
PROJ. No. I-95-9(12)

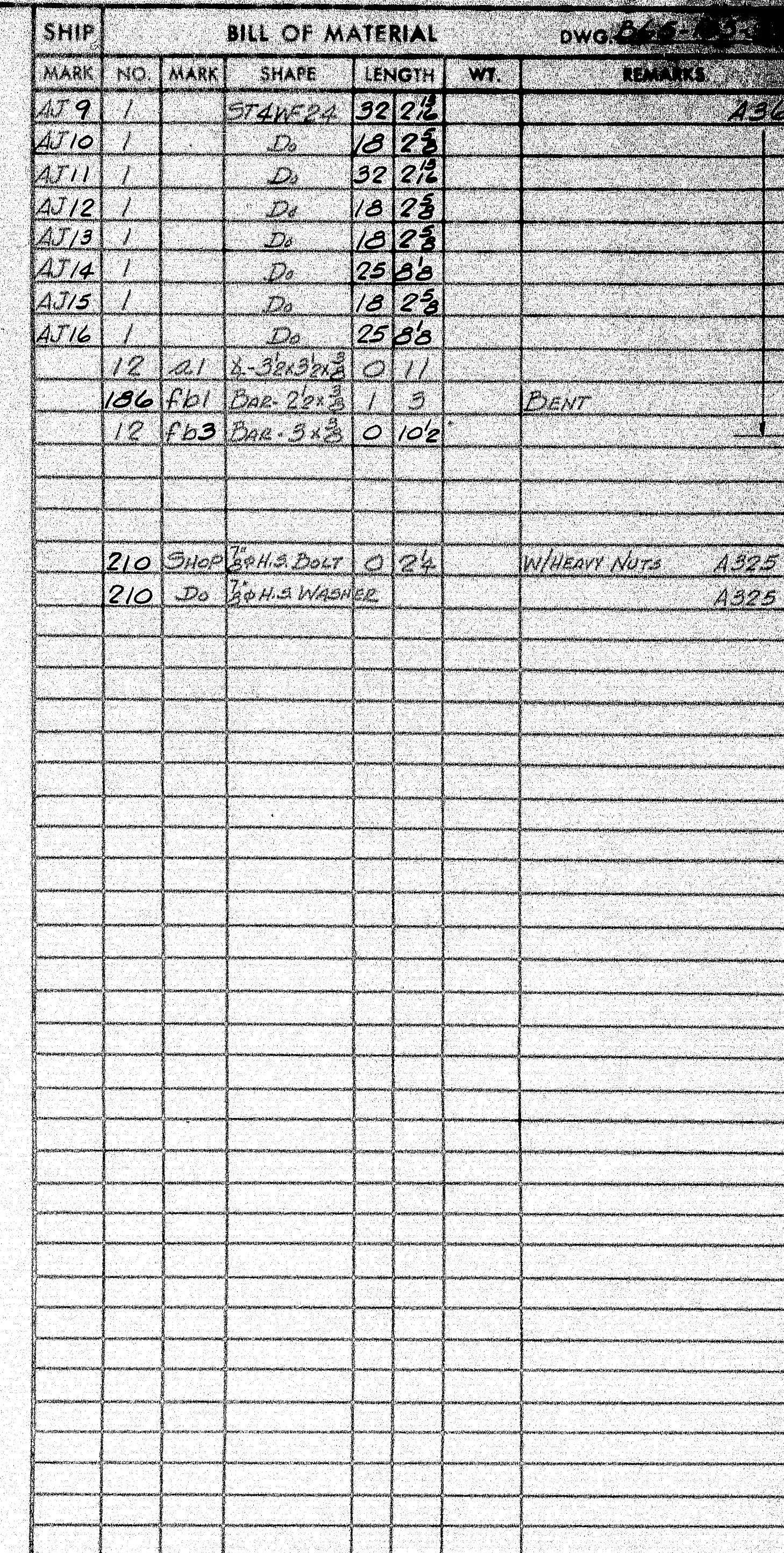
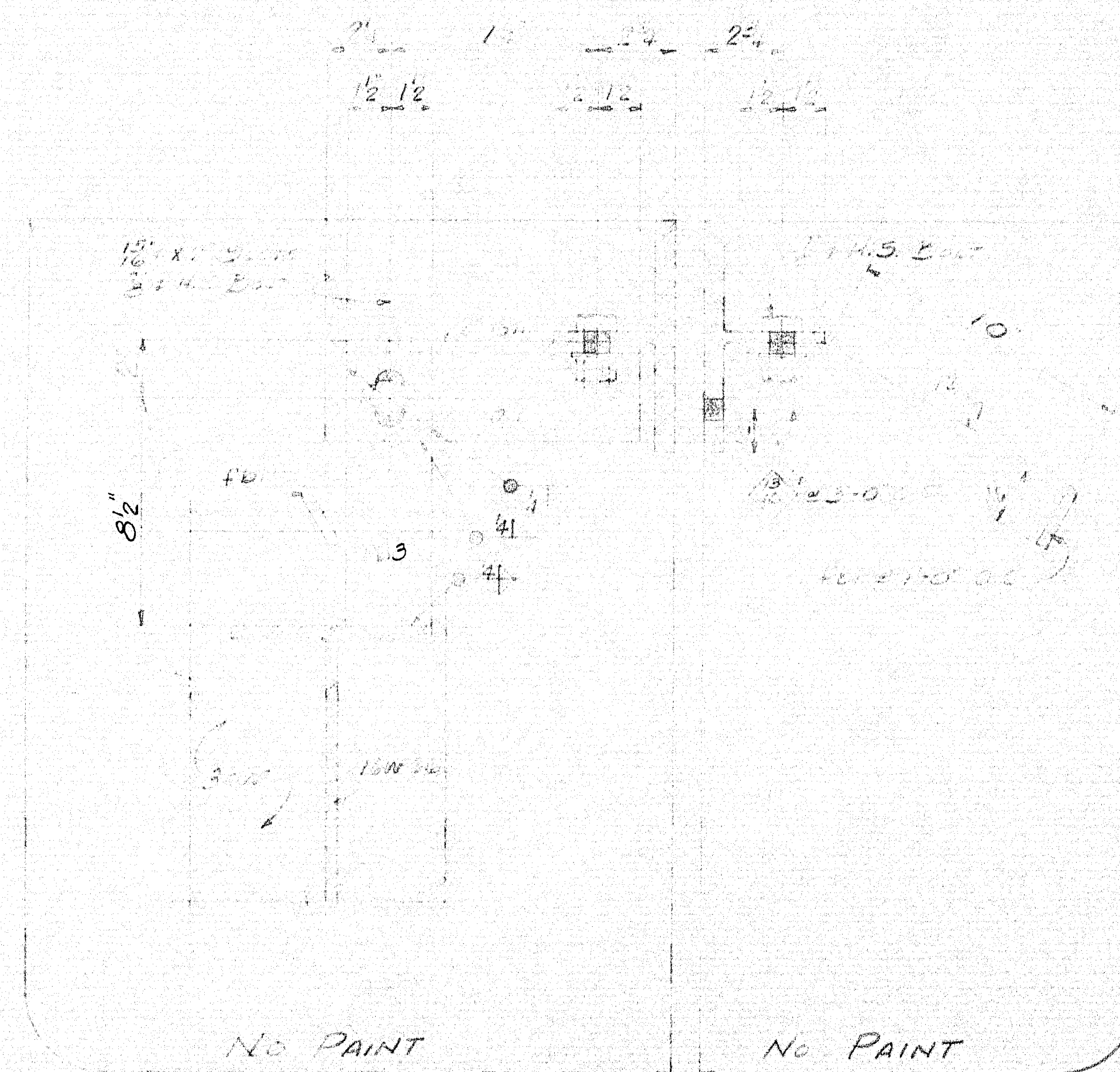
App'd. As NOTED 8-10-65
App'd. 7-29-65

BEARING PEDESTALS & ANCHOR BOLTS SOUTHBOUND

PRINT ISSUE		<p><i>Bancroft & Martin Inc.</i></p> <p><i>Brewer, Maine</i></p> <p>I-95 OVER E. BRANCH MATTANAMKEAGS RIVER</p> <p>OAKFIELD, MAINE</p>
5	J.H.C. 9-10-65	
3	CUST. 9-10-65	
4	PORT. 9-8-65	
2	F.A. 8-6-65	
4	PORT. 7-28-65	<p>CUSTOMER <u>CIANCHETTE BRO'S, INC.</u></p> <p>DESIGNER <u>M.S.H.C., BRIDGE DIVISION</u></p>
2	F.A. 7-28-65	
DRAWN	7-28-65 R.A.M.	
REVISION	8-4-65 R.A.M.	
REVISION		
REVISION		<p>ORDER <u>VERBAL</u></p> <p>DWG. <u>B65-135-51</u></p>

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SHOP CONNECTIONS: 3" H.S. BOLTS
FIELD CONNECTIONS: WELD
HOLES: 1 1/2" U.N.
PAINT: STATE OF MAINE SPEC.

APP'D. AS NOTED 8-10-65

Proj. No. I-95-9(12)

ARMORED JOINTS

NORTHBOUND

PRINT ISSUE

Rancroft & Martin Inc.
Brewer, Maine

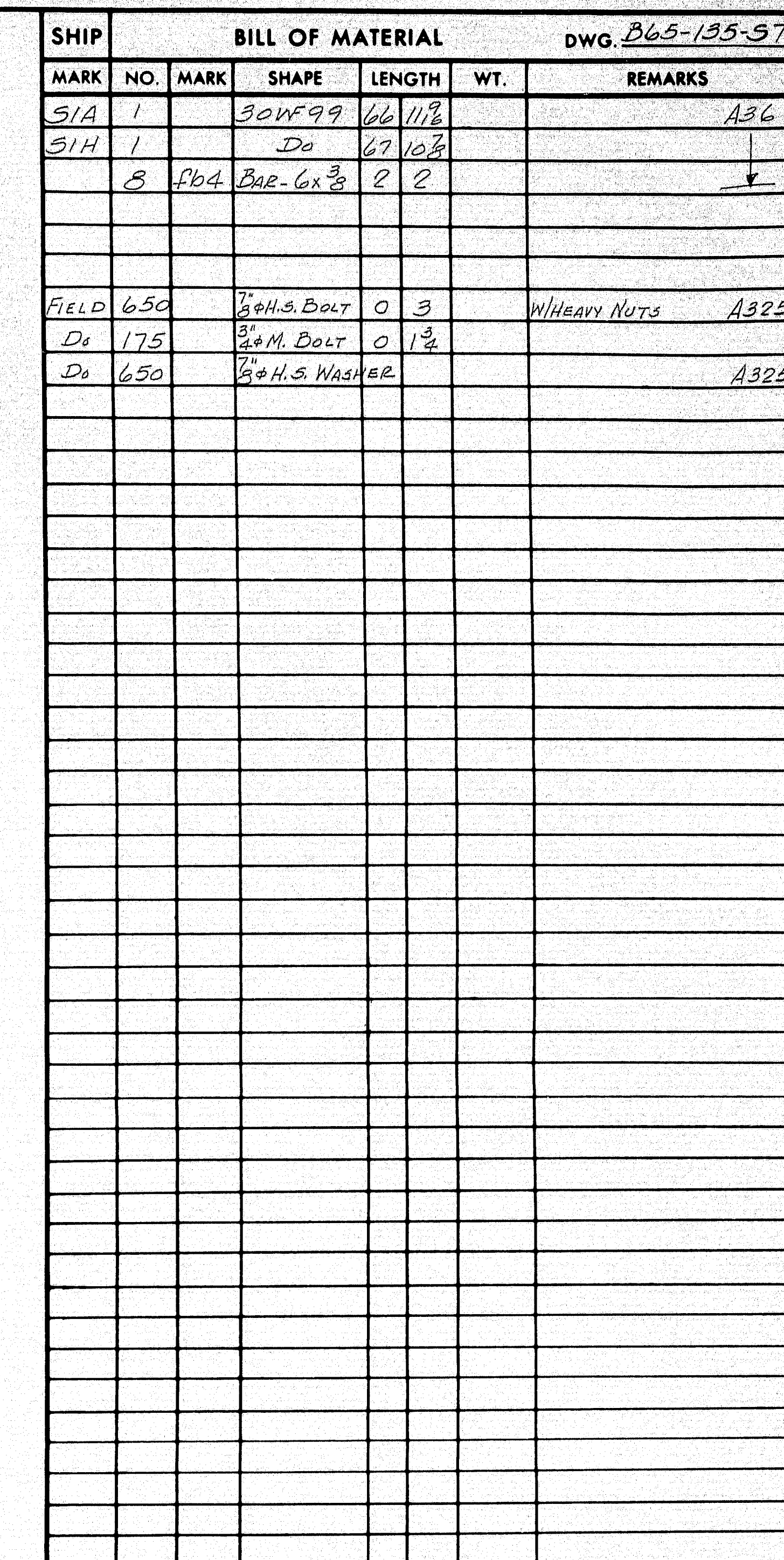
5	S.H.C.	9-1-65	I-95 OVER E. BRANCH MATTAWAMBEAG RIVER
3	CUST.	9-1-65	OAKFIELD,
5	SNRP	9-1-65	MAINE

2	F.A.	B-6-65	CUSTOMER	CIANCHETTE BROS., INC.
DRAWN	B-6-65	P.A.M.	DESIGNER	M.S.H.C., BRIDGE DIVISION
DESIGNER				

ORDER <u>VERBAL</u>	DWG. <u>B65-135-S6</u>
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SHOP CONNECTIONS: LH-E7018 or LH-E7028
FIELD CONNECTIONS: 3" ϕ H. S. BOLT
HOLES: 1 1/2" ϕ
PAINT: STATE OF MAINE SPEC'S.

APP'D. 8-25-65

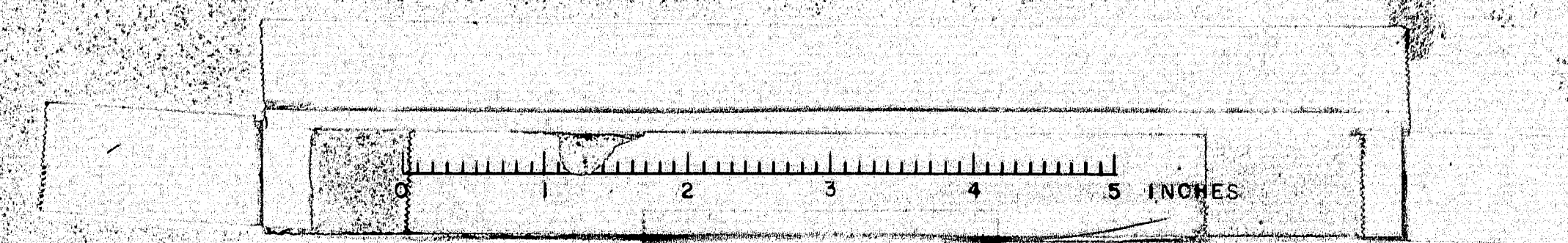
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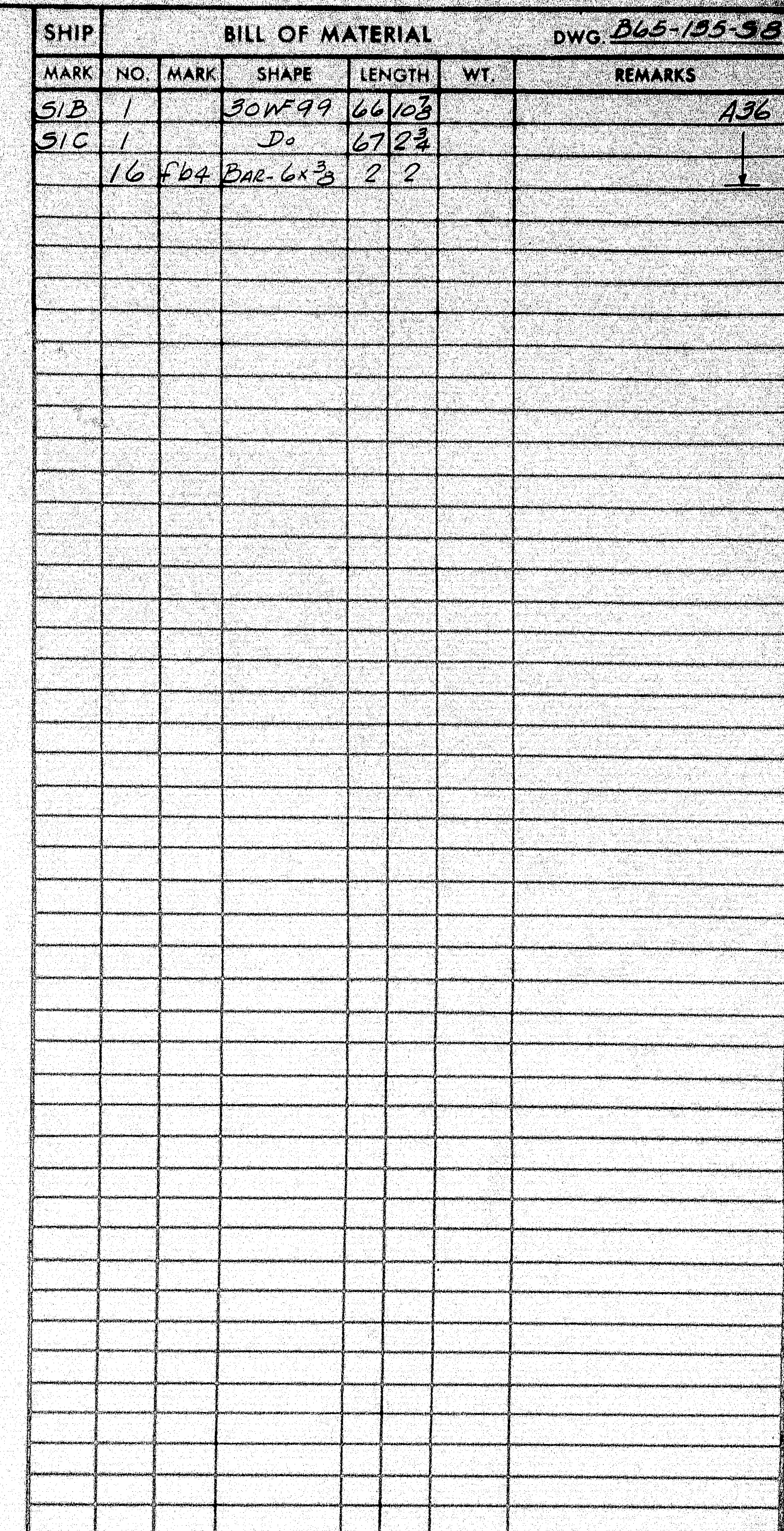
STRINGERS

SOUTHBOUND

PRINT ISSUE			<i>Bancroft & Martin Inc.</i> <i>Brewer, Maine</i>	
5	S.H.C.	9-10-65	I-95 OVER E. BRANCH MATTAWAMKEAG RIVER OAKFIELD, MAINE	
5	CUST	9-10-65		
5	SHOP	9-10-65		
2	F.A.	8-13-65	CUSTOMER <u>CIANCHETTE BROS., INC.</u> DESIGNER <u>M.S.H.C., BRIDGE DIVISION</u>	
DRAWN		8-9-65 R.A.H.		
REVISION				
REVISION				
REVISION			ORDER <u>VERBAL</u>	DWG. <u>B65-135-57</u>

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SHOP CONNECTIONS: LH-E701B or LH-E702B
FIELD CONNECTIONS: 3" x H.S. BOLT
HOLES: 1 1/2" ϕ
PAINT: STATE OF MAINE SPEC'S.

APP'D. 8-25-65

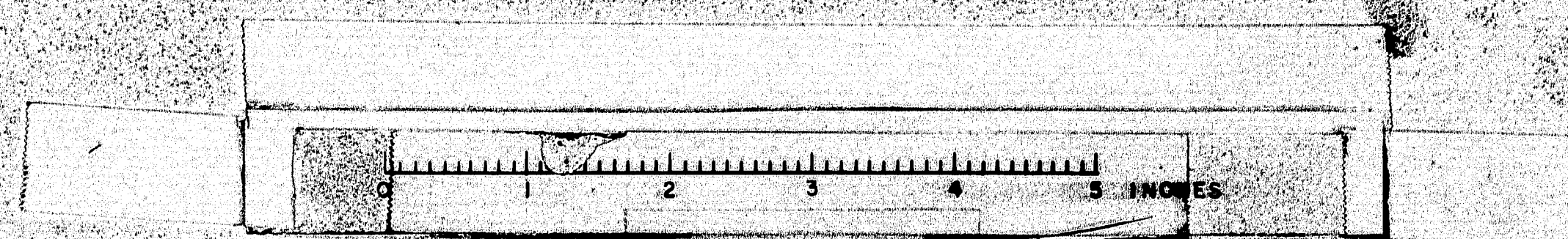
PROJ. No. I-95-9(12)

STRINGERS

SOUTHBOUND

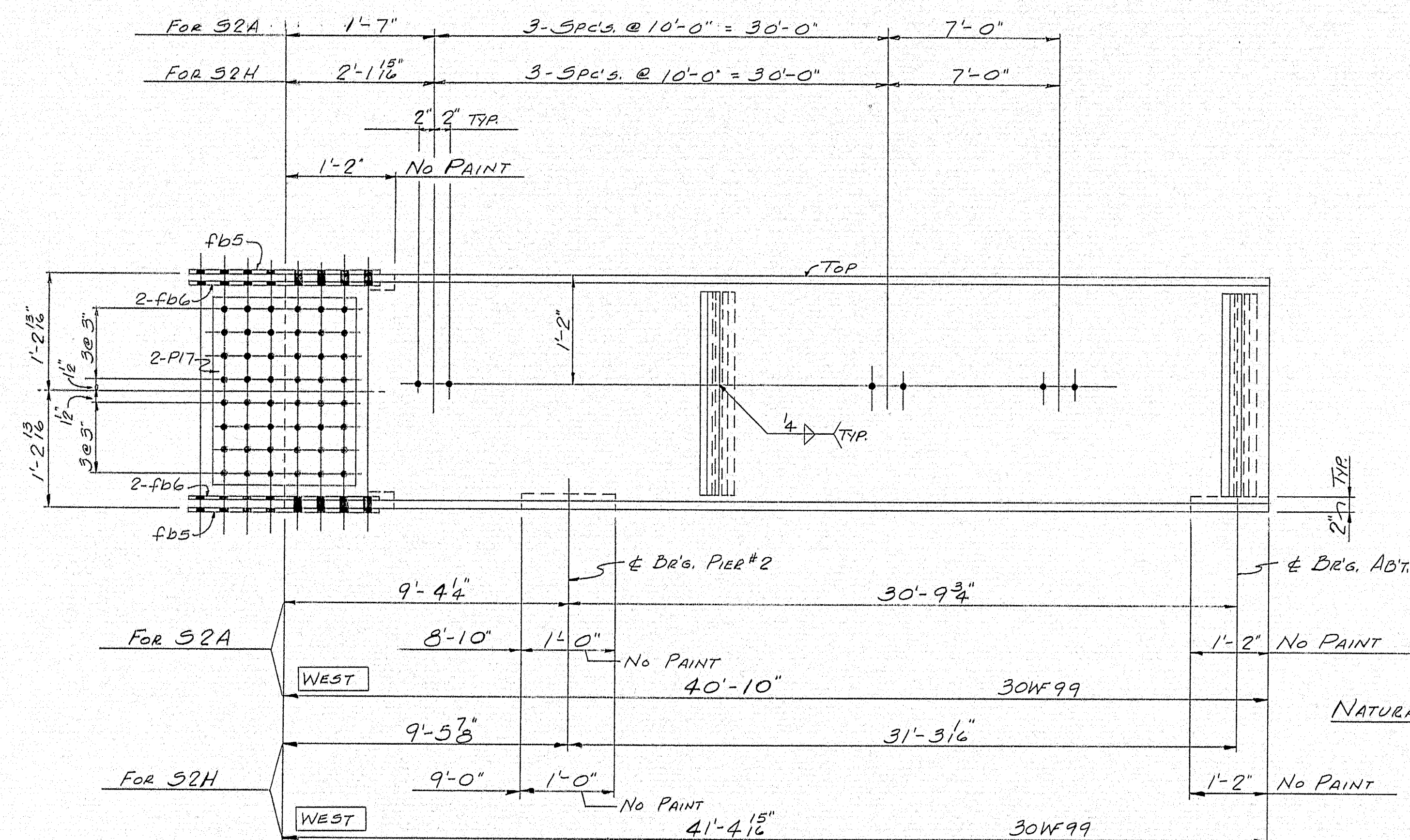
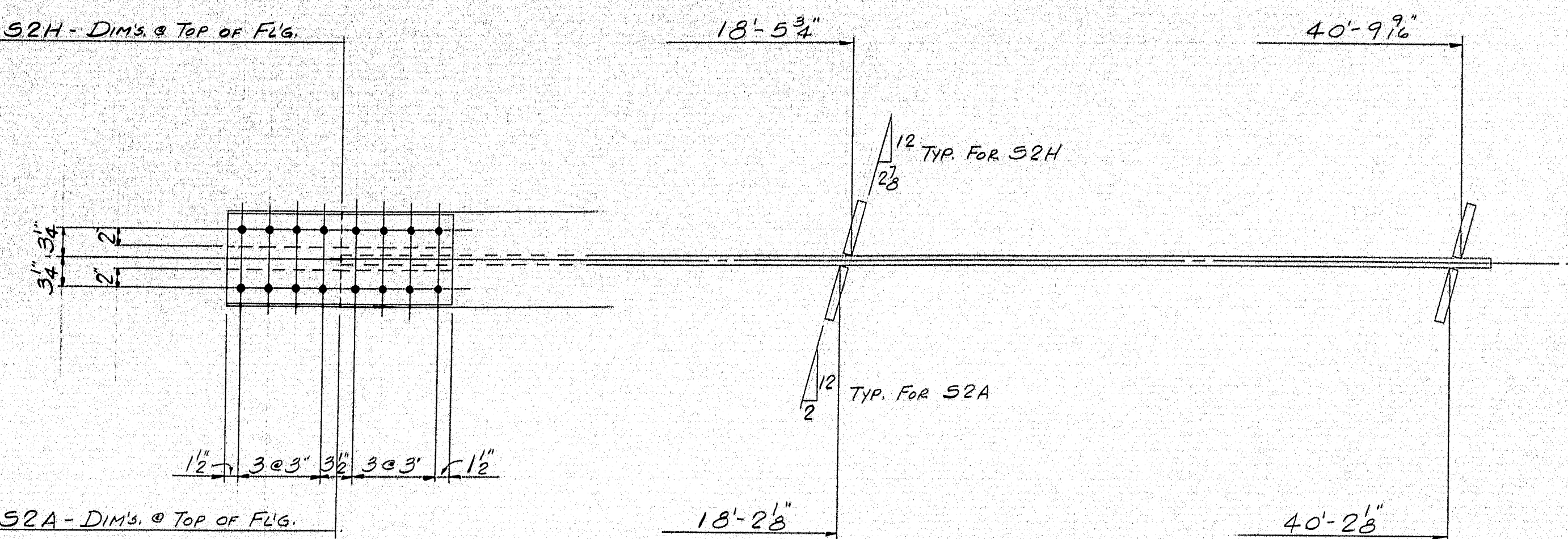
PRINT ISSUE			<i>Bancroft & Martin Inc.</i> <i>Brewer, Maine</i>
5	J.H.C.	9-10-65	I-95 OVER E. BRANCH MATTAWAMKEAG RIVER OAKFIELD, MAINE
5	CUST.	9-10-65	
5	SHOP	9-10-65	
2	I.F.A.	8-13-65	
DRAWN 8-10-65 R.A.M.			CUSTOMER CIANCHETTE BROS., INC. DESIGNER M.S.H.C., BRIDGE DIVISION
REVISION			
REVISION			
REVISION			
			ORDER <u>VERBAL</u> DWG. <u>B65-135-58</u>

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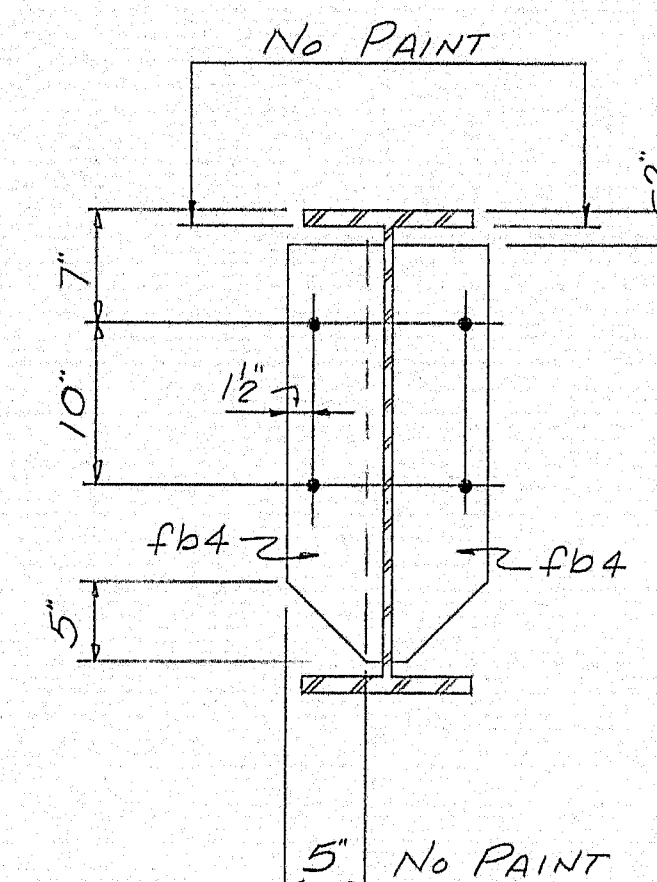
For S2H - DIMS. & TOP OF F.L.G.

For S2A - DIMS. & TOP OF F.L.G.



ONE - S2A

ONE - S2H



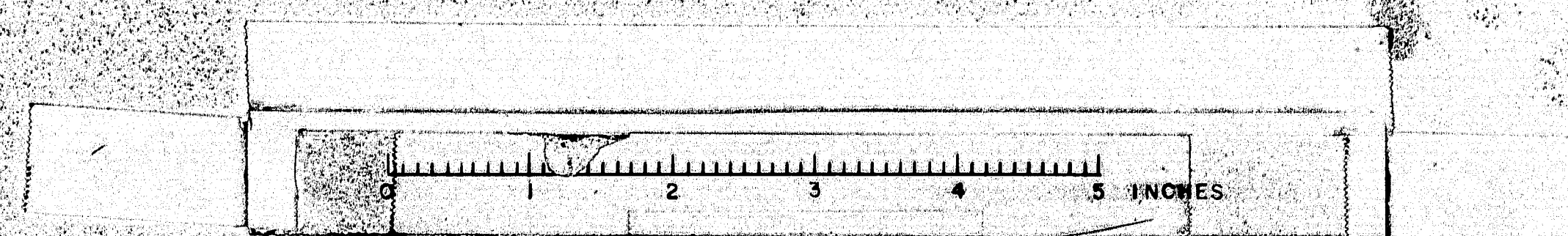
SHOP CONNECTIONS: LH-E7013 or LH-E7023
FIELD CONNECTIONS: 3/8\"/>

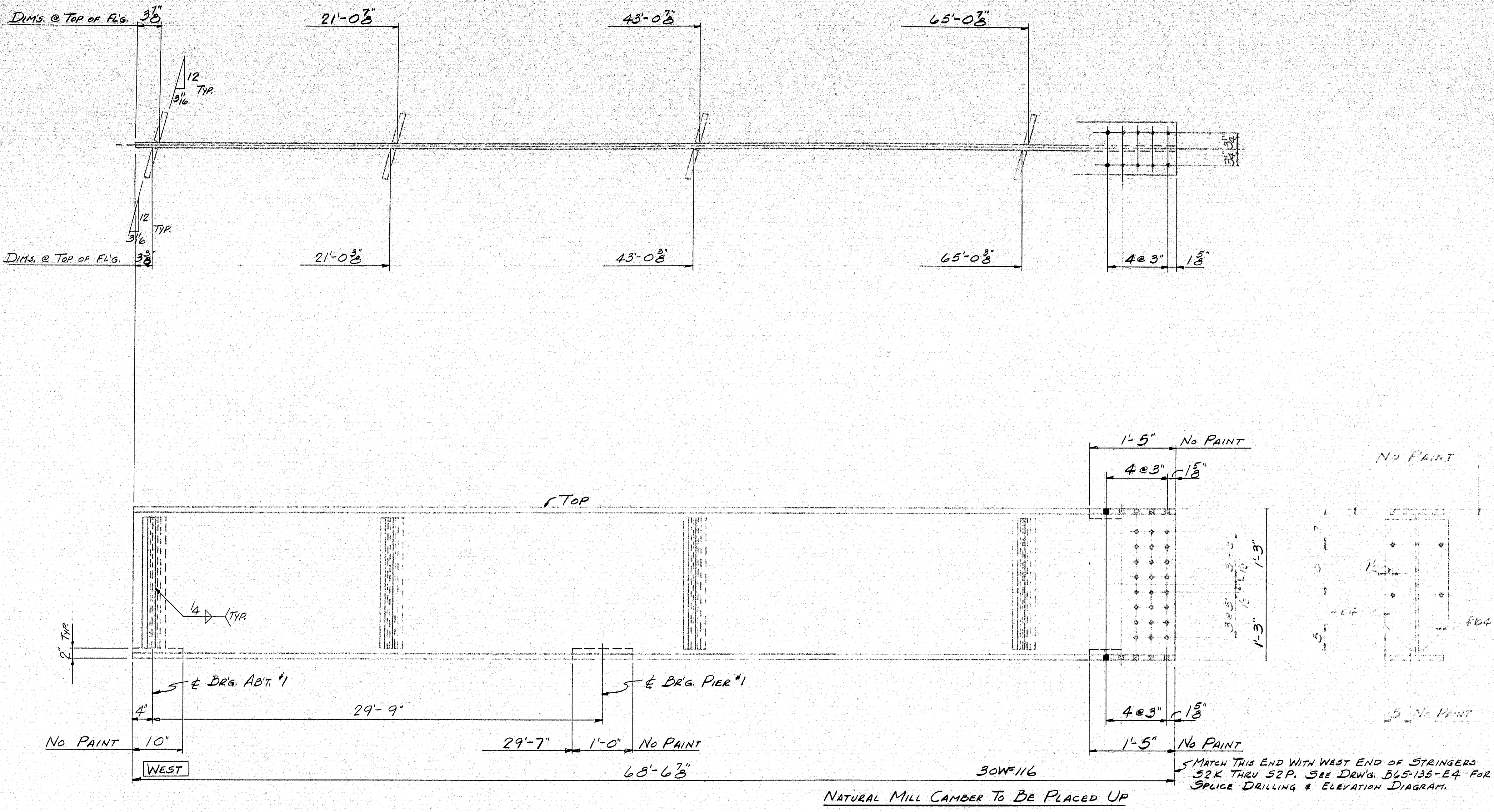
APP'D. 8-25-65

PROJ. No. I-95-9 (12)

STRINGERS		SOUTHBOUND	
PRINT ISSUE		Bancroft & Martin Inc. Brewer, Maine	
5	S.H.C. 9-10-65	I-95 OVER E. BRANCH MATTAWAMKEAG RIVER	
3	CUST. 9-10-65	OAKFIELD, MAINE	
5	SHOP 9-10-65		
2	F.A. 8-13-65		
DRAWN 8-10-65 R.A.M.		CUSTOMER CIANCHETTE BROS., INC.	
REVISION		DESIGNER M.S.H.C. BRIDGE DIVISION	
REVISION		ORDER VERBAL	
REVISION		DWG. B65-135-S10	

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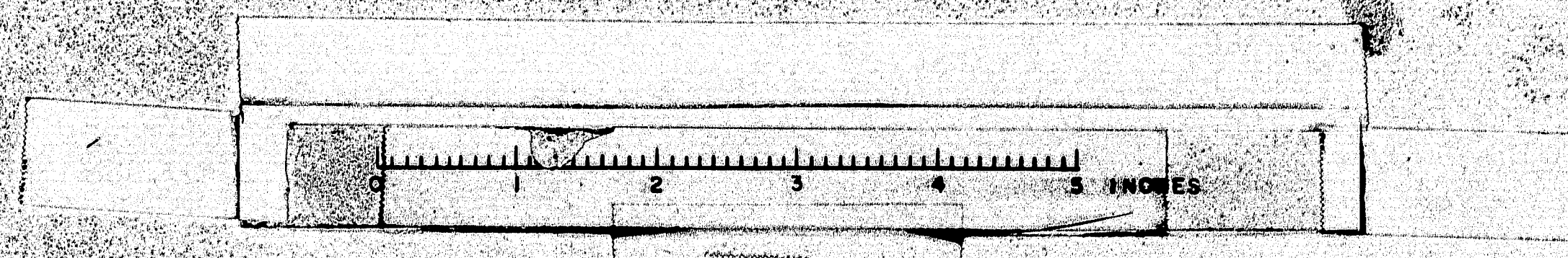
ONE - 31K
 ONE - 31M
 ONE - 31N
 ONE - 31P

SHIP		BILL OF MATERIAL				DWG. B65-135-514
MARK	NO.	MARK	SHAPE	LENGTH	WT.	REMARKS
31K	1		30W116	68'6 3/8"		A36
31M	1		D ₀	68'6 3/8"		
31N	1		D ₀	68'6 3/8"		
31P	1		D ₀	68'6 3/8"		
	32	Fb4	DAR-613	2'2"		

SHOP CONNECTIONS: L4-E7023 & L4-E7028
 FIELD CONNECTIONS: 3" H S BOLT
 HOLES: 1 1/2" Ø
 PAINT: STATE OF MAINE SPEC'S.

APP'D. 8-25-65		PROJ. No. I-95-9(12)	
STRINGERS		NORTHBOUND	
PRINT ISSUE		Bancroft & Martin Inc. Brewer, Maine	
5 S.H.C.	9-10-65	I-95 OVER E. BRANCH MATTAWAMKEAG RIVER	
3 CUST.	9-10-65	OAKFIELD, MAINE	
5 SHOP	9-10-65	CUSTOMER CIANCHETTE BROS., INC.	
2 F.A.	8-13-65	DESIGNER M.S.H.C. BRIDGE DIVISION	
DRAWN	8-11-65 R.A.M.	ORDER VERBAL	
REVISION		DWG. B65-135-514	
REVISION			
REVISION			

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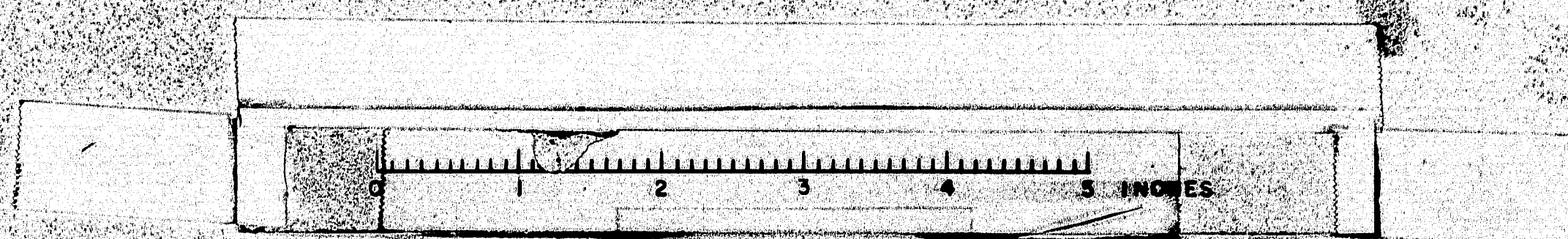
30WF116

5 MATCH THIS END WITH WEST END OF STRINGER
G2R. SEE DRWG. B65-135-E4 FOR SPLICE
DRILLING & ELEVATION DIAGRAM.

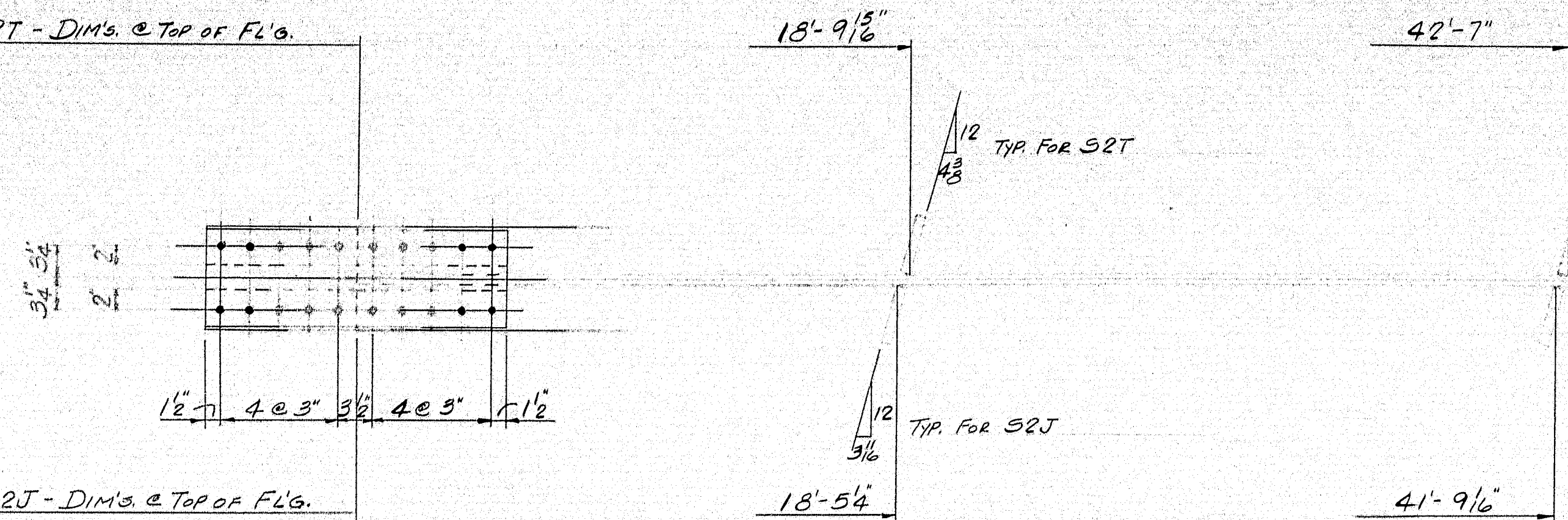
SHOP CONNECTIONS: *LH-E701B or LH-E702B*
FIELD CONNECTIONS: *3" x H.S. BOLT*
HOLES: *1 1/2" x*
PAINT: *STATE OF MAINE SPEC'S.*

Proj. No. I-95-9(12)

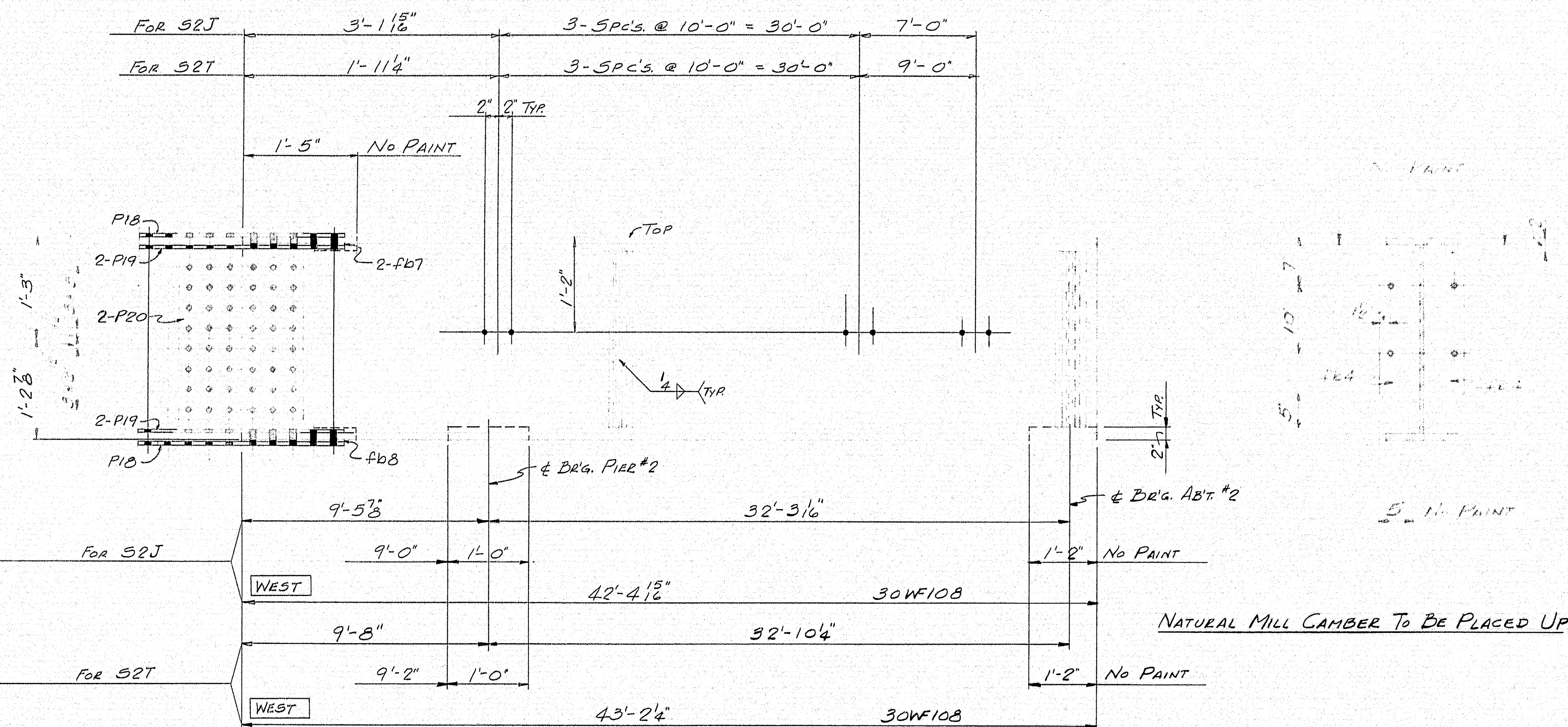
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FOR S2T - DIMS. @ TOP OF FLG.



FOR S2J - DIMS. @ TOP OF FLG.



ONE - S2J

ONE - S2T

SHIP		BILL OF MATERIAL				DWG. 865-135-S16
MARK	NO.	MARK	SHAPE	LENGTH	WT.	REMARKS
S2J	1		30W108	42' 4 1/2"		A36
S2T	1		D	43' 2 1/4"		
	4	fb4	BAR-6x3	2	2	
	4	fb7	BAR-4x3	1	3 1/2	
	2	fb8	BAR-10x3	1	3 1/2	
	4	P18	R-10x1/6	2	6 1/2	
	8	P19	R-4x1/6	2	6 1/2	
	4	P20	R-18x1/6	2	0	

SHOP CONNECTIONS: LH-E7018 or LH-E7028

FIELD CONNECTIONS: 3" H.S. BOLT

HOLES: 1/8" #

PAINT: STATE OF MAINE SPEC'S

APPD. 8-25-65

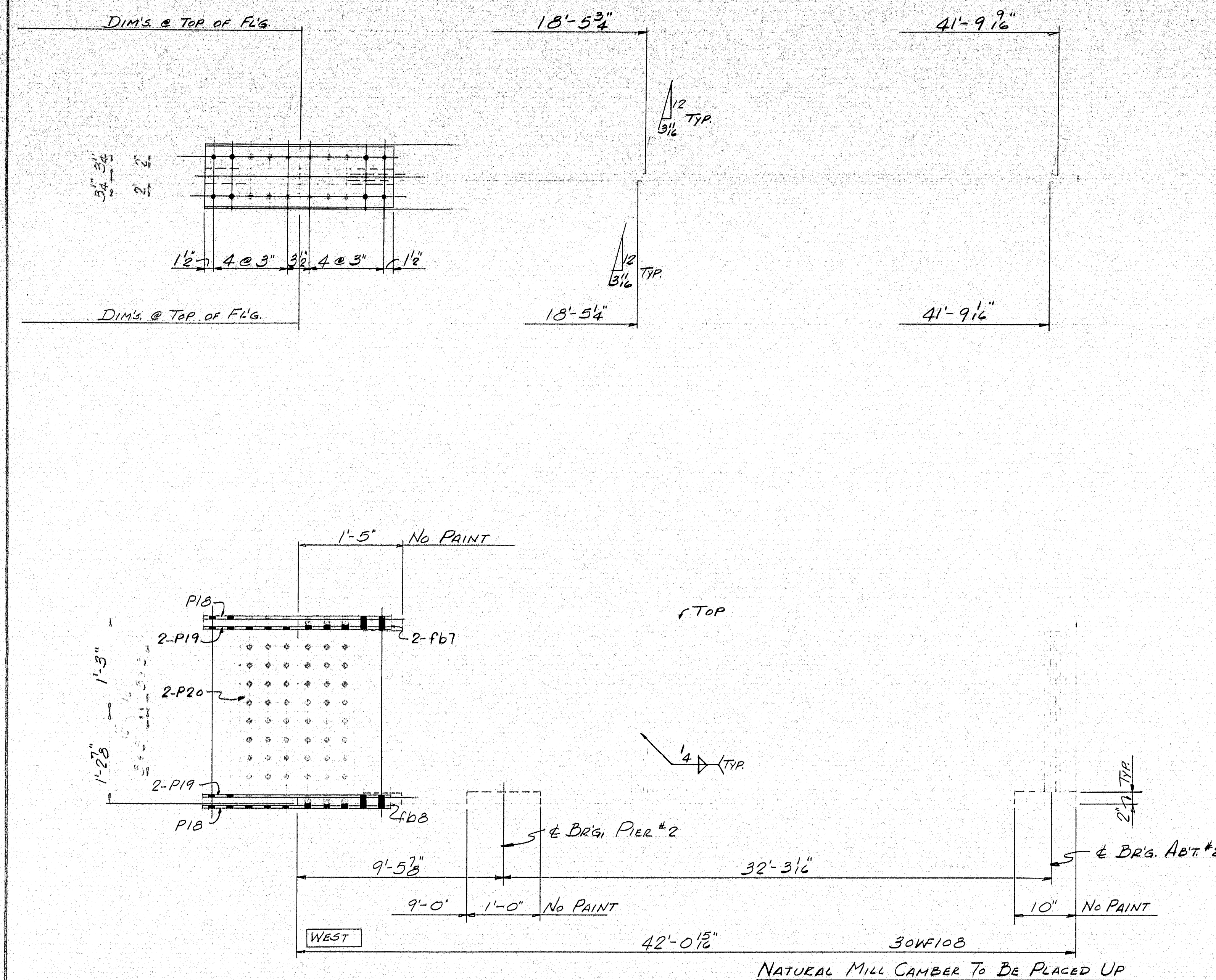
PROJ. No. I-95-9 (12)

STRINGERS

NORTHBOUND

PRINT ISSUE		BRANCH & MARTIN INC.	
		Brewer, Maine	
5	S.H.C. 9-10-65	I-95 OVER E. BRANCH MATTAWAMKEAG RIVER	
3	CUST. 9-10-65	OAKFIELD, MAINE	
5	SHOP 9-10-65		
2	F.A. 8-13-65		
DRAWN	8-12-65 R.A.M.	CUSTOMER CIANCHETTE BROS., INC.	
REVISION		DESIGNER M.S.H.C. BRIDGE DIVISION	
REVISION		ORDER VERBAL	
REVISION		DWG. 865-135-S16	

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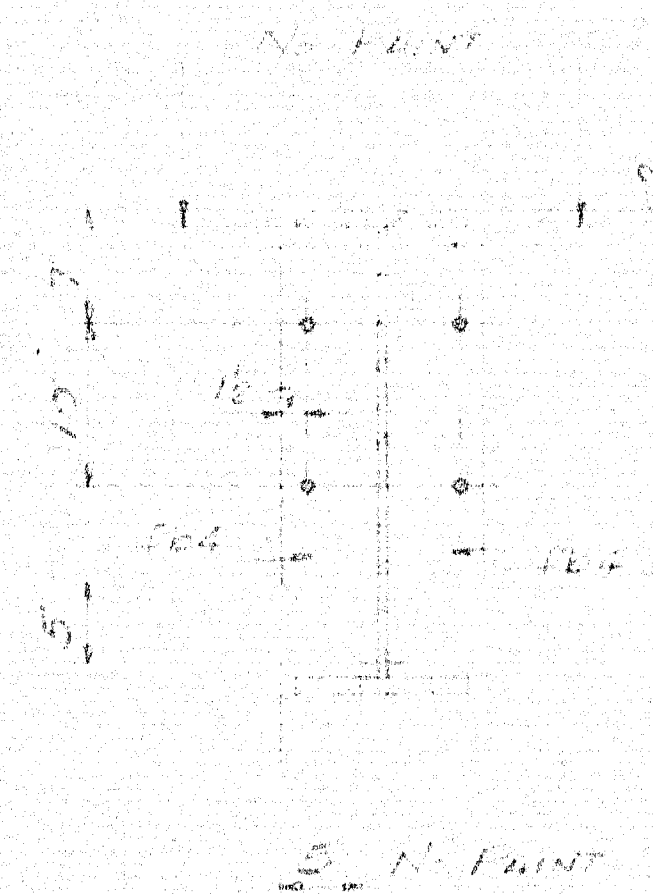
ONE - S2K
ONE - S2M
ONE - S2N
ONE - S2P

SHIP		BILL OF MATERIAL				DWG. B65-135-517
MARK	NO.	MARK	SHAPE	LENGTH	WT.	REMARKS
S2K	1		30W108	42' 0 1/2"		A36
S2M	1		D ₀	42' 0 1/2"		
S2N	1		D ₀	42' 0 1/2"		
S2P	1		D ₀	42' 0 1/2"		
	16	FD4	DAR-6x3	2	2	
	3	FD7	DAR-4x3	1	3 1/2	
	4	FD8	DAR-10x3	1	3 1/2	
	3	P13	R-10x16	2	6 1/2	
	16	P19	R-4x16	2	6 1/2	
	3	P20	R-13x16	2	0	

SHOP CONNECTIONS: LH-E7018 or LH-E7028
FIELD CONNECTIONS: 3/4" H.S. BOLT
HOLES: 1 1/8" Ø
PAINT: STATE OF MAINE SPEC'S.

APP'D. 8-25-65		PROJ. No. I-95-9 (12)	
STRINGERS		NORTHBOUND	
PRINT ISSUE		Bancroft & Martin Inc. Brewer, Maine	
5	S.H.C.	9-10-65	I-95 OVER E. BRANCH MATTAWAMKEAG RIVER
3	CUST.	9-10-65	OAKFIELD, MAINE
5	SHOP	9-10-65	
2	FA	8-13-65	
DRAWN	8-12-65	RAM.	CUSTOMER: CIANCHETTE BROS., INC.
REVISION			DESIGNER: M.S.H.C. BRIDGE DIVISION
REVISION			ORDER: VERBAL
REVISION			DWG. B65-135-517

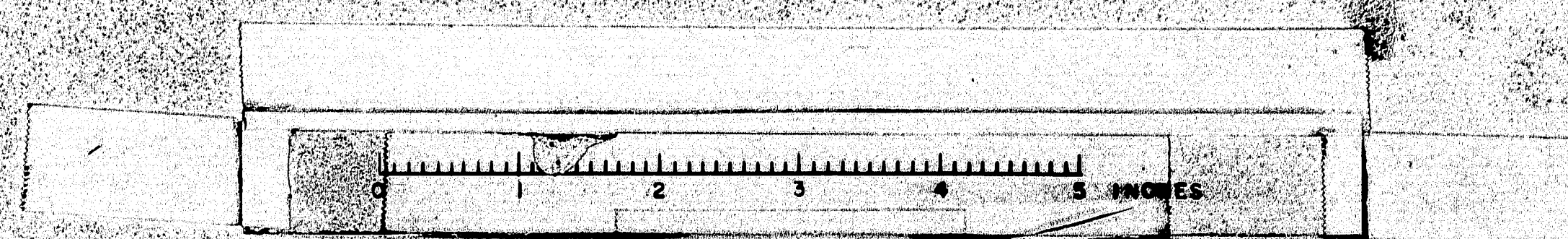
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SHOP CONNECTIONS: LH-E701B or LH-E702B
FIELD CONNECTIONS: 3" H.S. BOLT
HOLES: 1 5/16" ϕ
PAINT: STATE OF MAINE SPEC'S.

Proj. No. I-95-9 (12)

97-195



SPECIFICATIONS:

A.A.S.H.O. Standard Specifications for Highway Bridges 1961 with Interim Specifications, 1961, 1962, 1963 & 1964.

CONTRACT:

State of Maine, State Highway Commission Standard Specifications for Highways and Bridges, Revision of January 1956 and Supplemental Specifications of February 1960.

LIVE LOADING

H-20-44 (Modified for Interstate)

ALLOWABLE STRESSES

Concrete (n=10) ~ f_c = 1200 p.s.i.
Reinforcing Steel, Int. Grade ~ f_s = 20,000 p.s.i.
Structural Steel ~ f_s = 20,000 p.s.i. (A.S.T.M. Designation A-36).

CONCRETE CLASSIFICATION

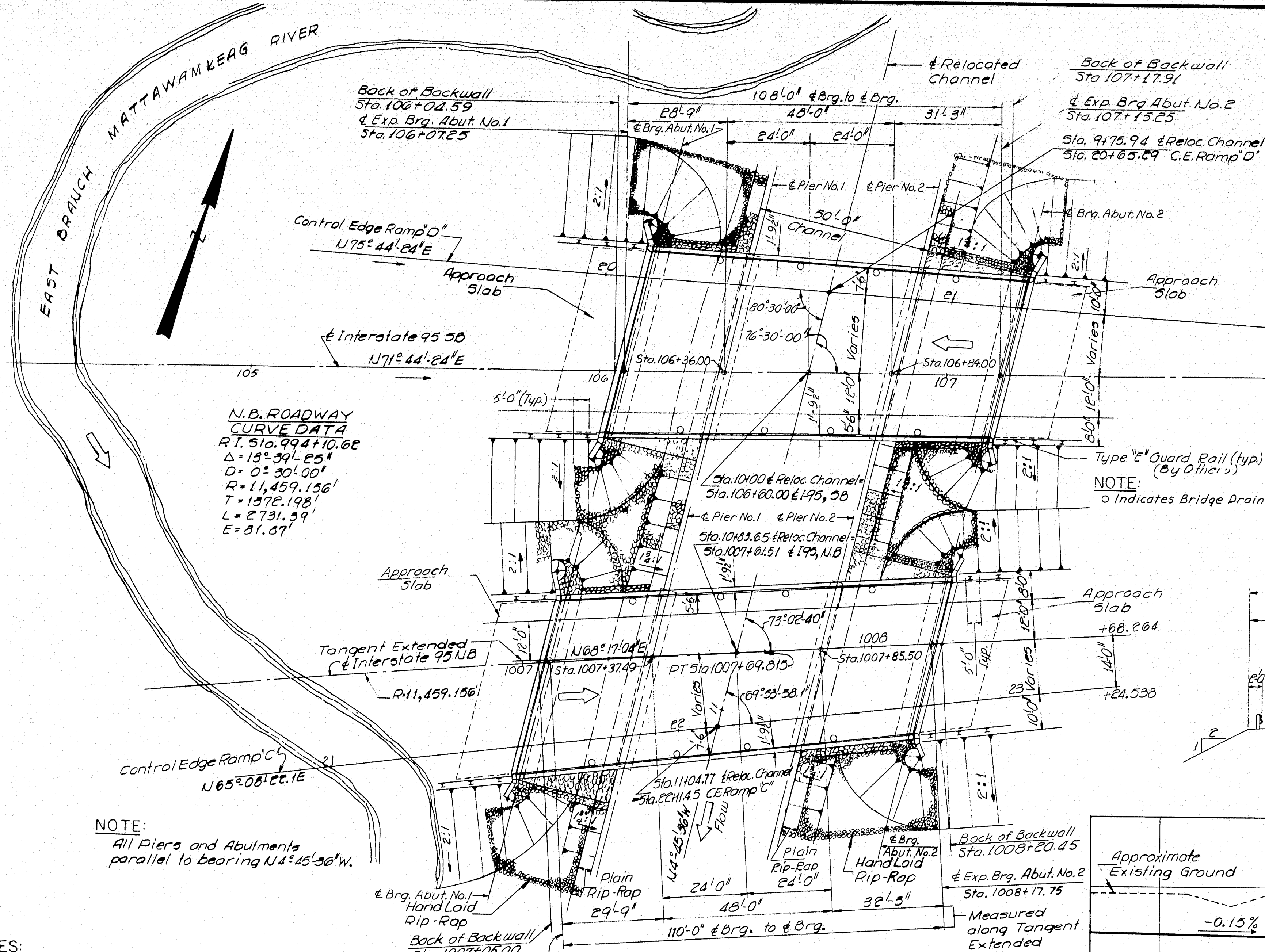
All Concrete shall be Class "A".

FOUNDATIONS

Abutments 10BP42 End Bearing Piles (Capacity 37 Tons)
Piers 10BP42 End Bearing Piles (Capacity 37 Tons)

HYDRAULIC DATA

A = 48.1 Sq. Miles
S = 22.85 Ft/Mile
Q₅₀ = 3435 c.f.s.

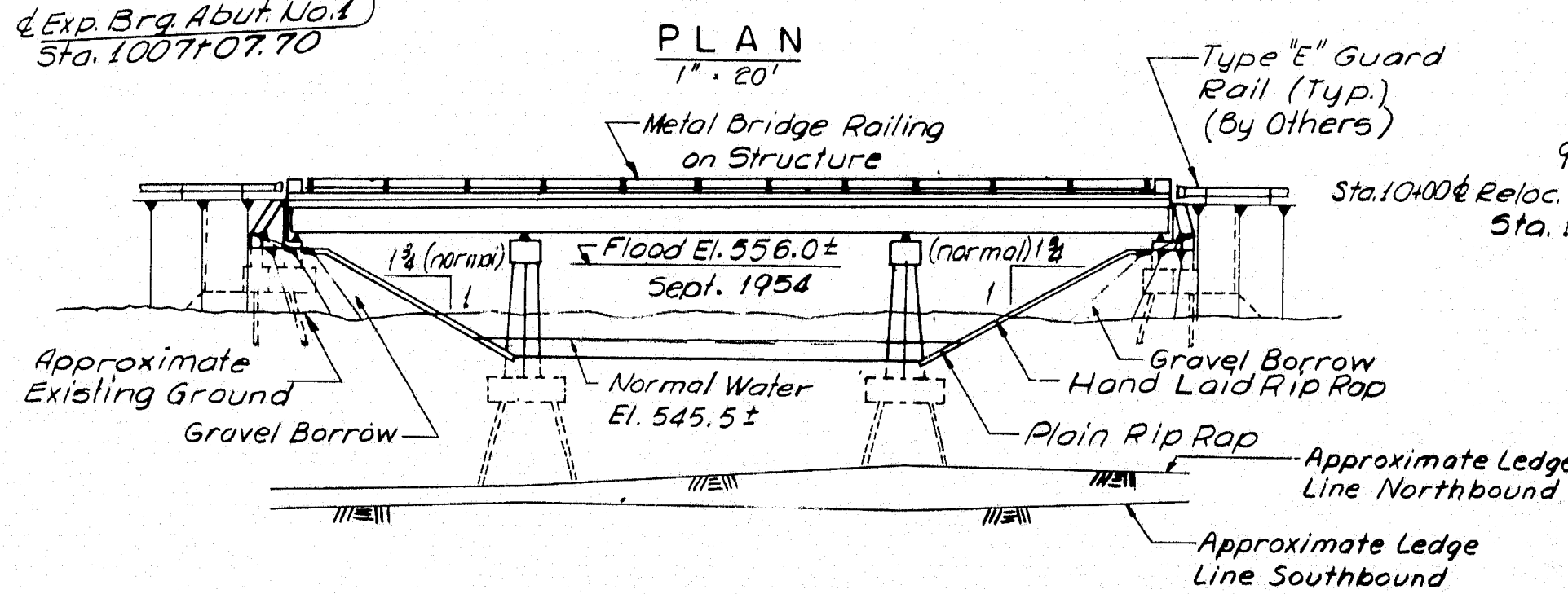


NOTES:

- All fill within the limits shown on the profile sheet shall be placed by the controlled density method.
- Size of stone in gravel borrow through which abutment piles are driven should not exceed 6 inches and concentrations of stones in the area shall be avoided.
- Place gravel borrow to elevation of abutment footings before driving piles.

INDEX OF SHEETS

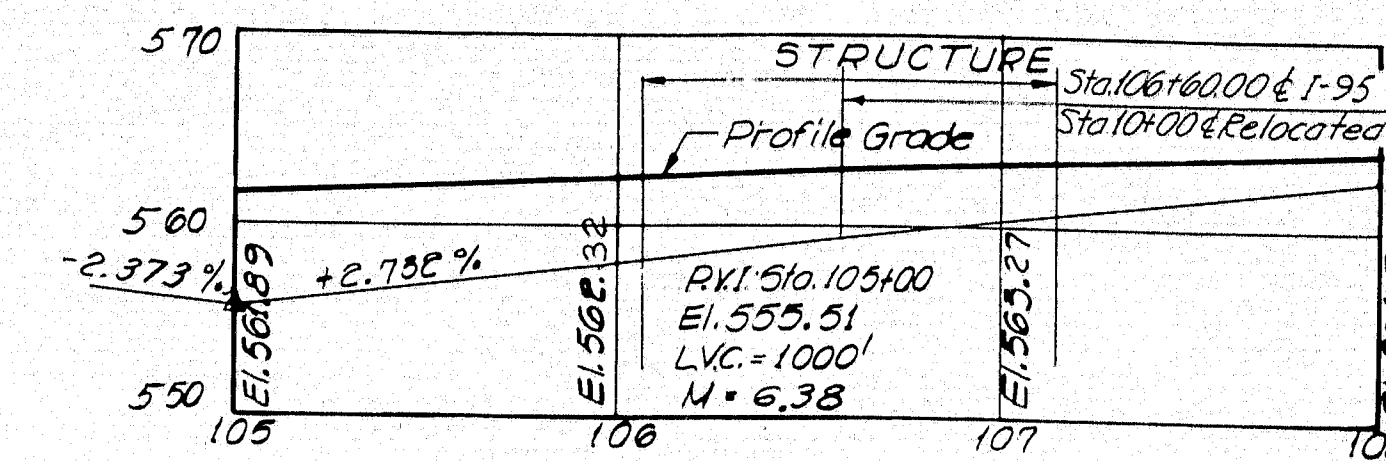
- GENERAL PLAN & QUANTITIES
- FOUNDATION SURVEY, SB.
- FOUNDATION SURVEY, NB.
- ABUTMENT NO. 1 SB.
- ABUTMENT NO. 2 SB. & APPROACH SLAB
- ABUTMENT NO. 1 NB. & APPROACH SLAB
- ABUTMENT NO. 2 NB.
- PIERS SB.
- PIERS NB.
- STRUCTURAL STEEL & BLOCKING SB.
- STRUCTURAL STEEL & BLOCKING NB.
- SUPERSTRUCTURE SB.
- SUPERSTRUCTURE NB.
- SLOPE PROTECTION
- REINFORCING STEEL
- REINFORCING STEEL

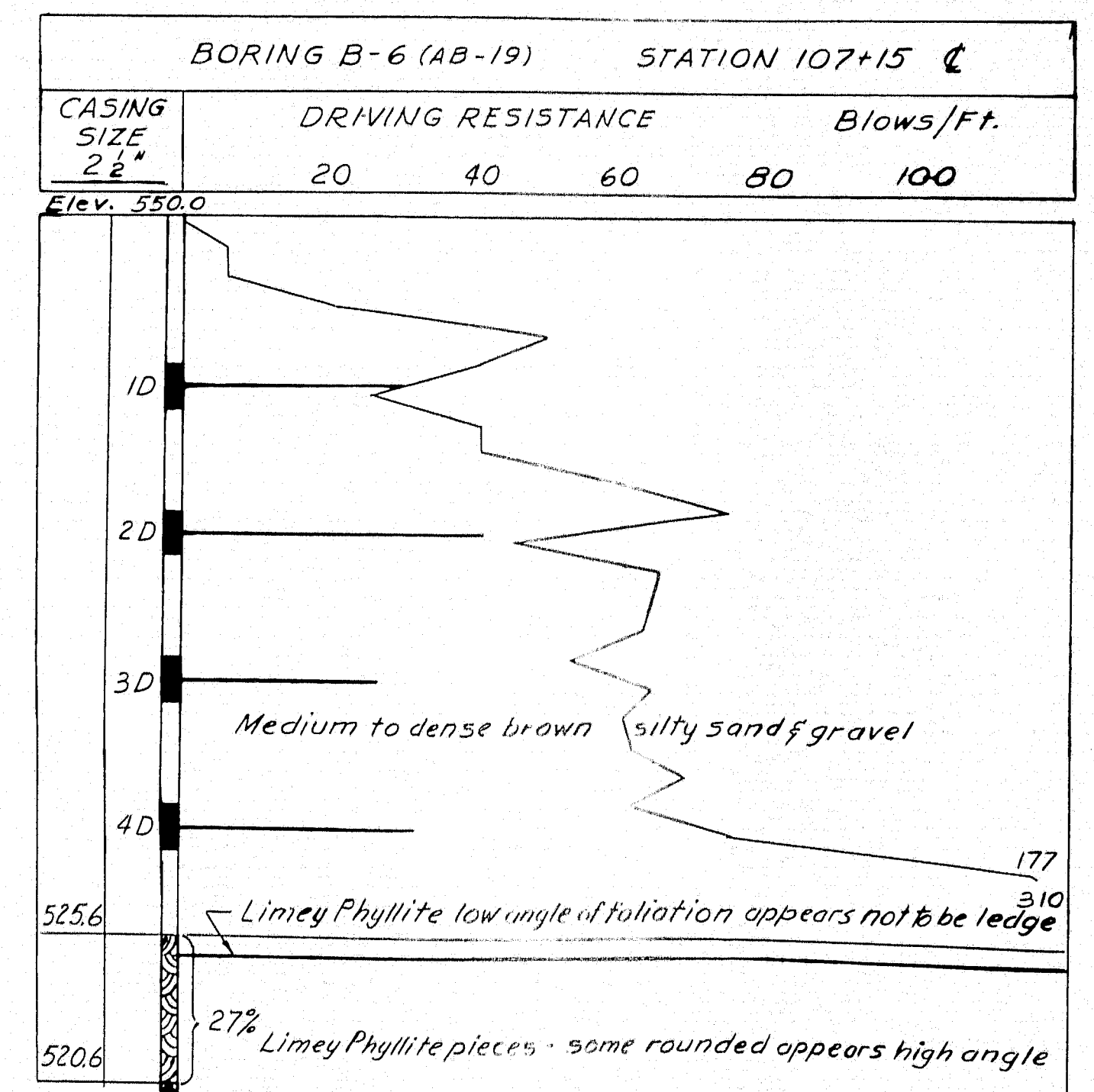
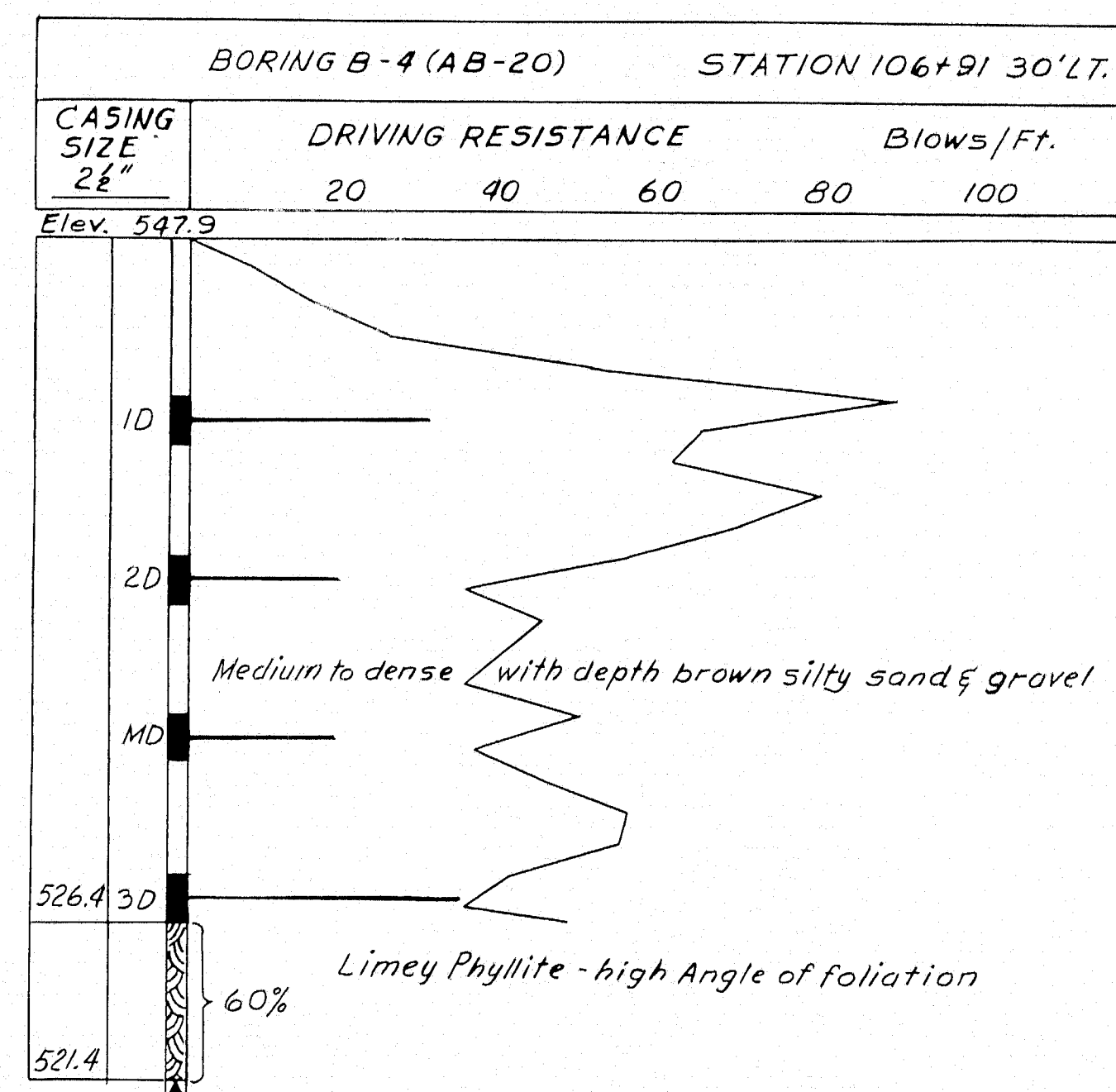
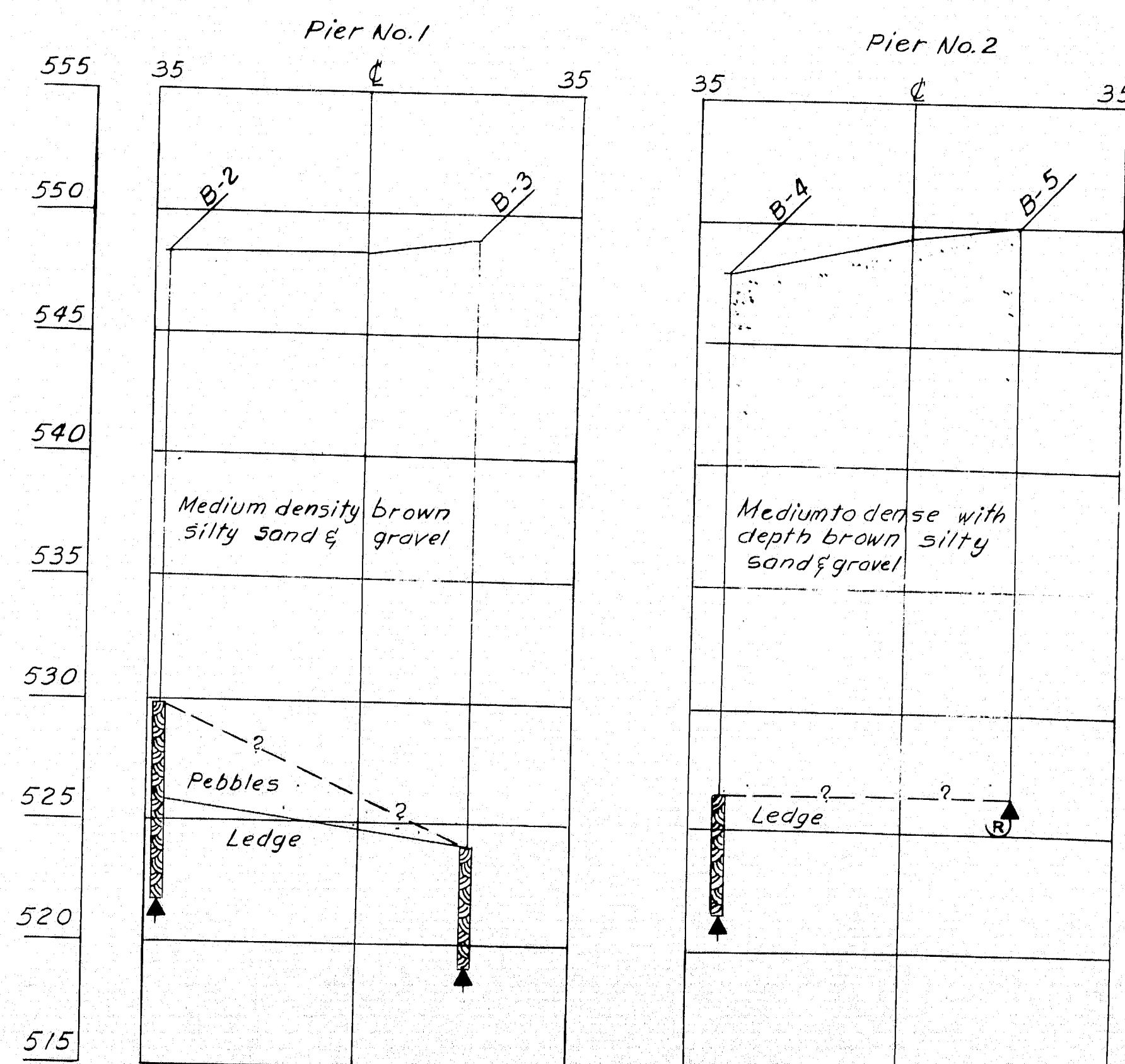
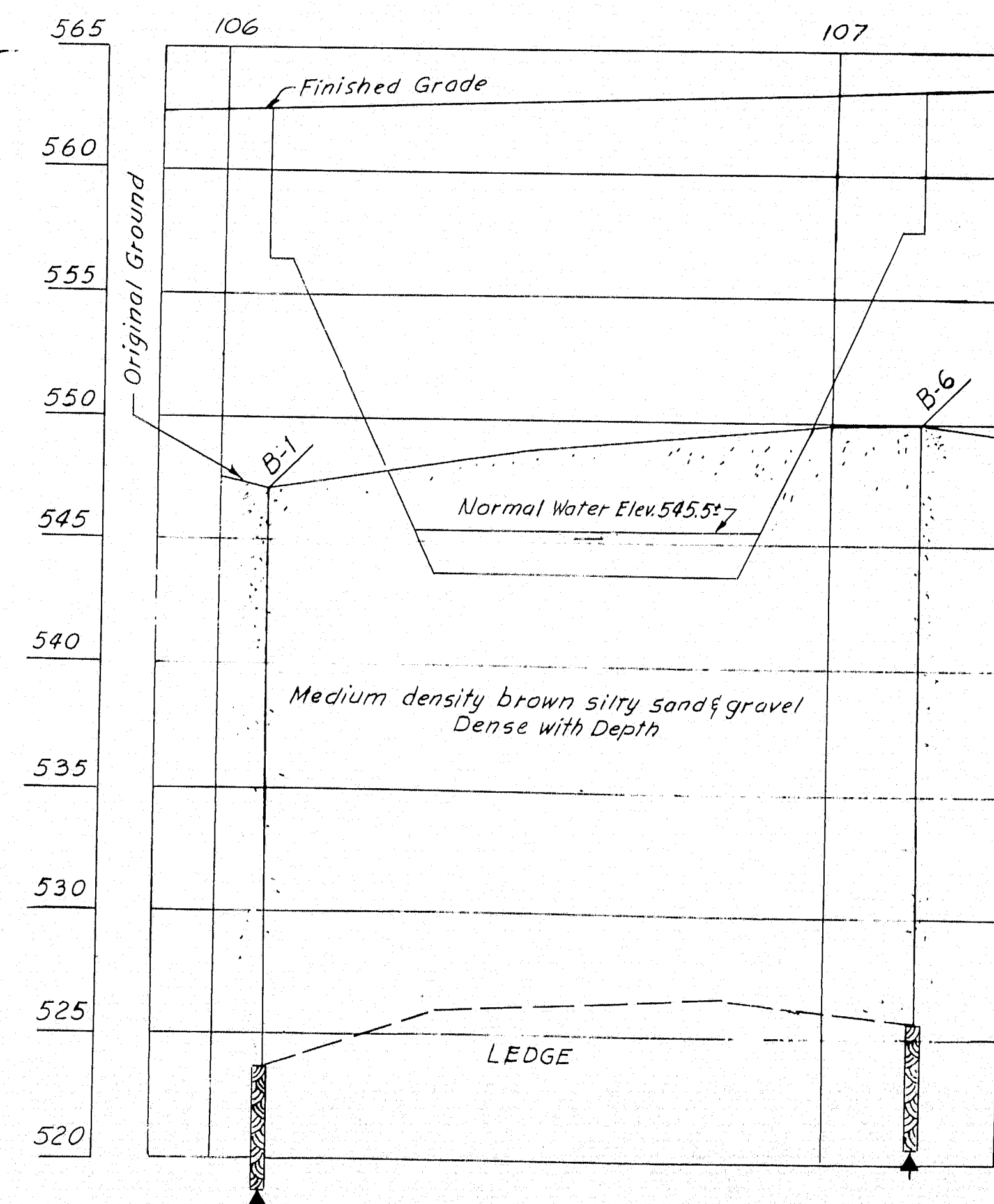
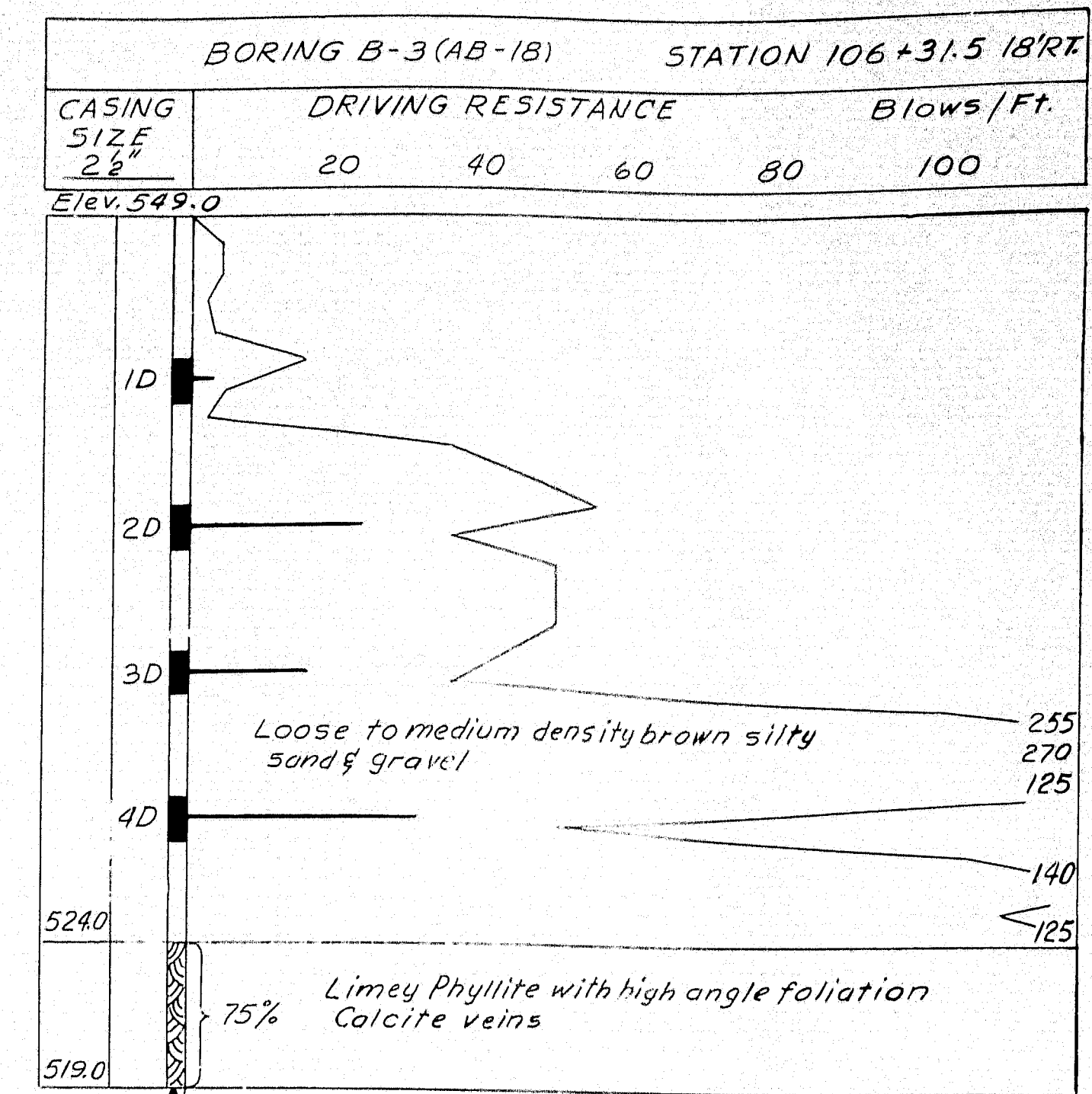
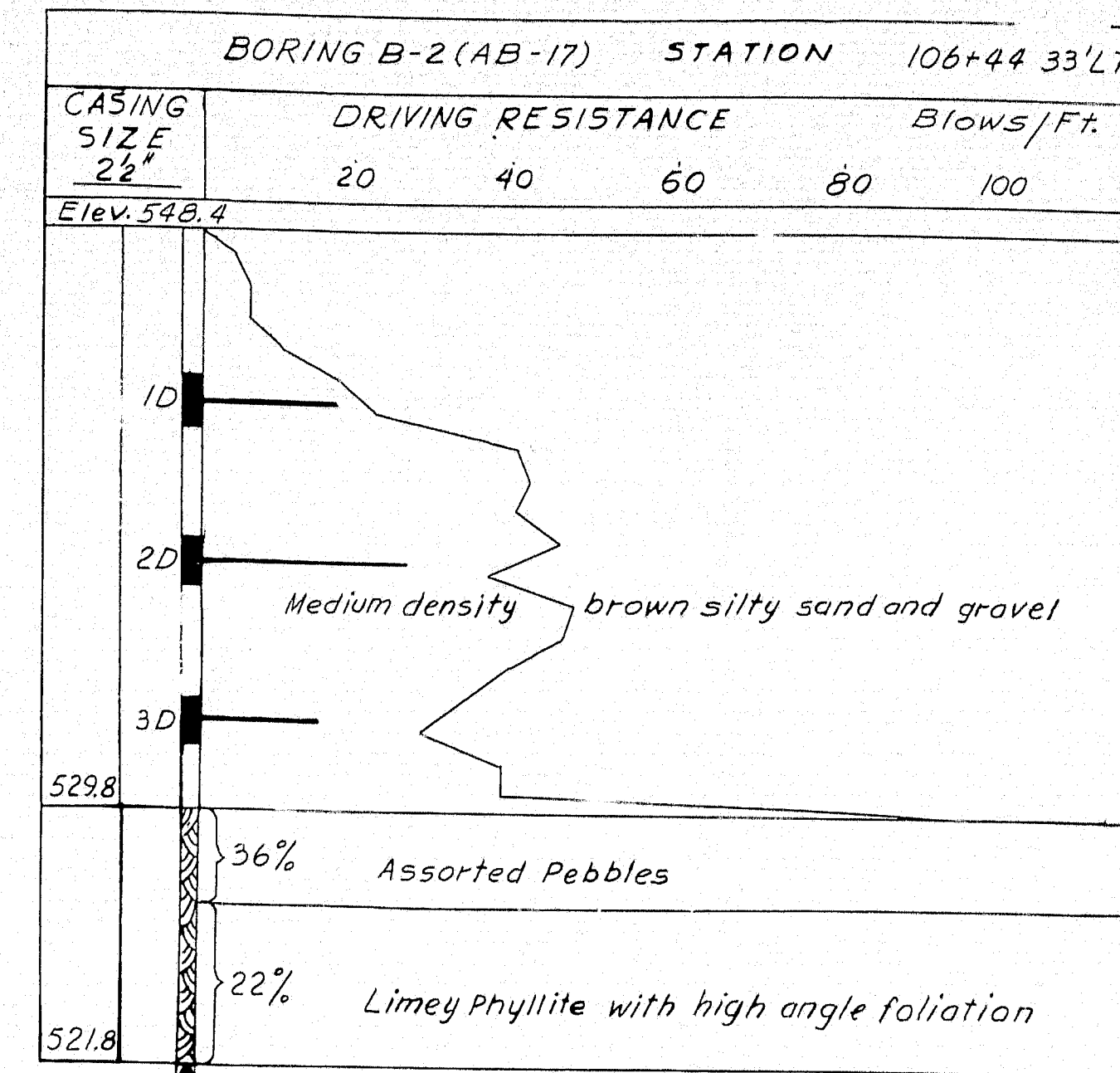
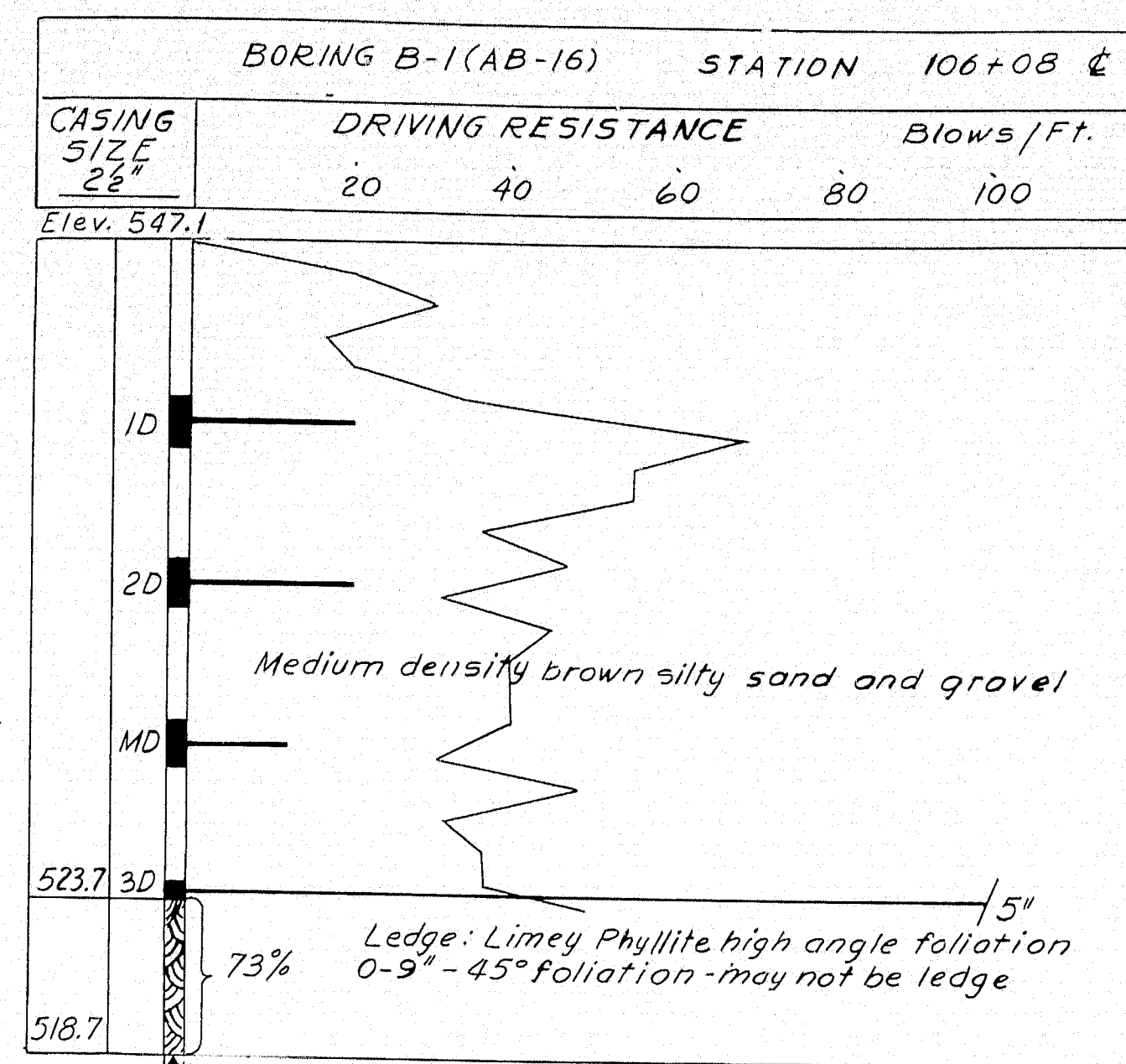
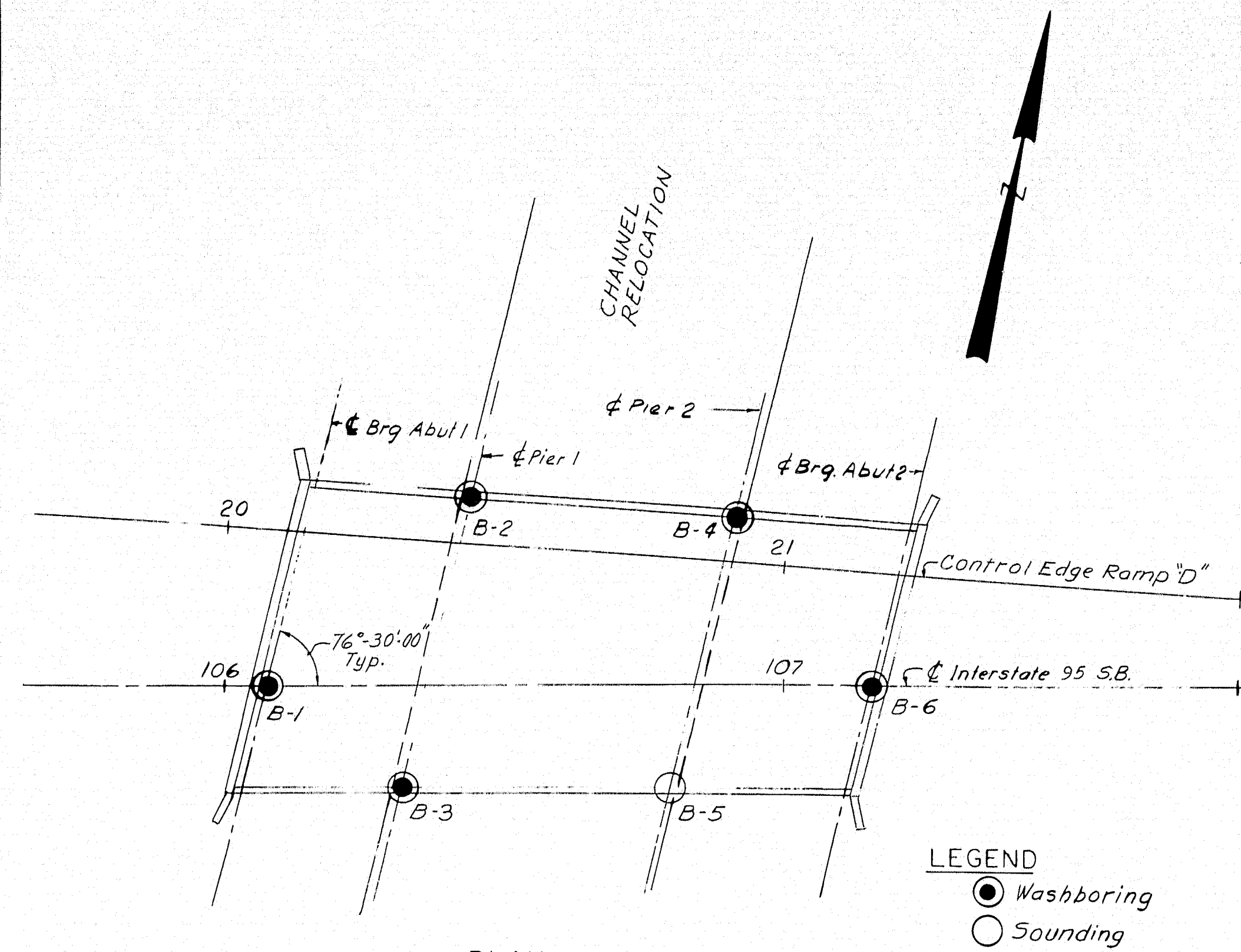


ELEVATION

STANDARD DETAIL SHEETS

- | | |
|----------|---------------------------|
| BD101-64 | BEARING PEDESTALS |
| BD103-64 | BEAM SPICES |
| BD104-64 | DIAPHRAGMS, ARMORED JOINT |
| BD107-64 | SHEAR CONNECTORS, DRAIN |
| BD108-64 | STEEL RAIL |
| BD109-64 | ALUMINUM RAIL |





- NOTES:
- Number of blows required to drive extra heavy casing one foot with 400 ft. lbs of energy per blow.
 - Location of sample, or sample attempt.
 - 5.8H Sampler #1290's unsuccessful sample attempt and type of sampler
 - Number of blows required to drive spoon or tubing one foot with 350 ft. lbs. of energy per blow.
 - Bottom of boring (may not be bottom of soil strata)
 - Refusal of drill rods or casing (may not be ledge)
 - 7 1/2" Locations cored by diamond bit and per cent recovery of rock.

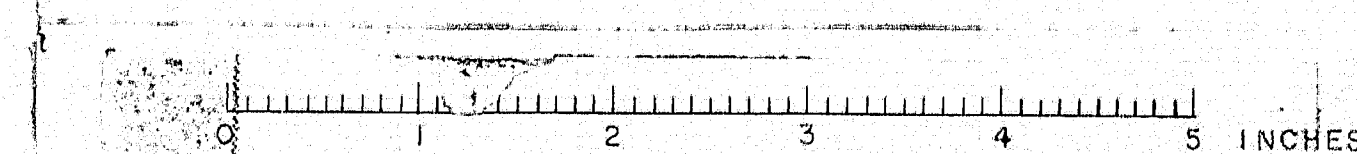
HOWARD, NEEDLES, TAMMEN & BERGENDOFF
CONSULTING ENGINEERS
NEW YORK BOSTON KANSAS CITY

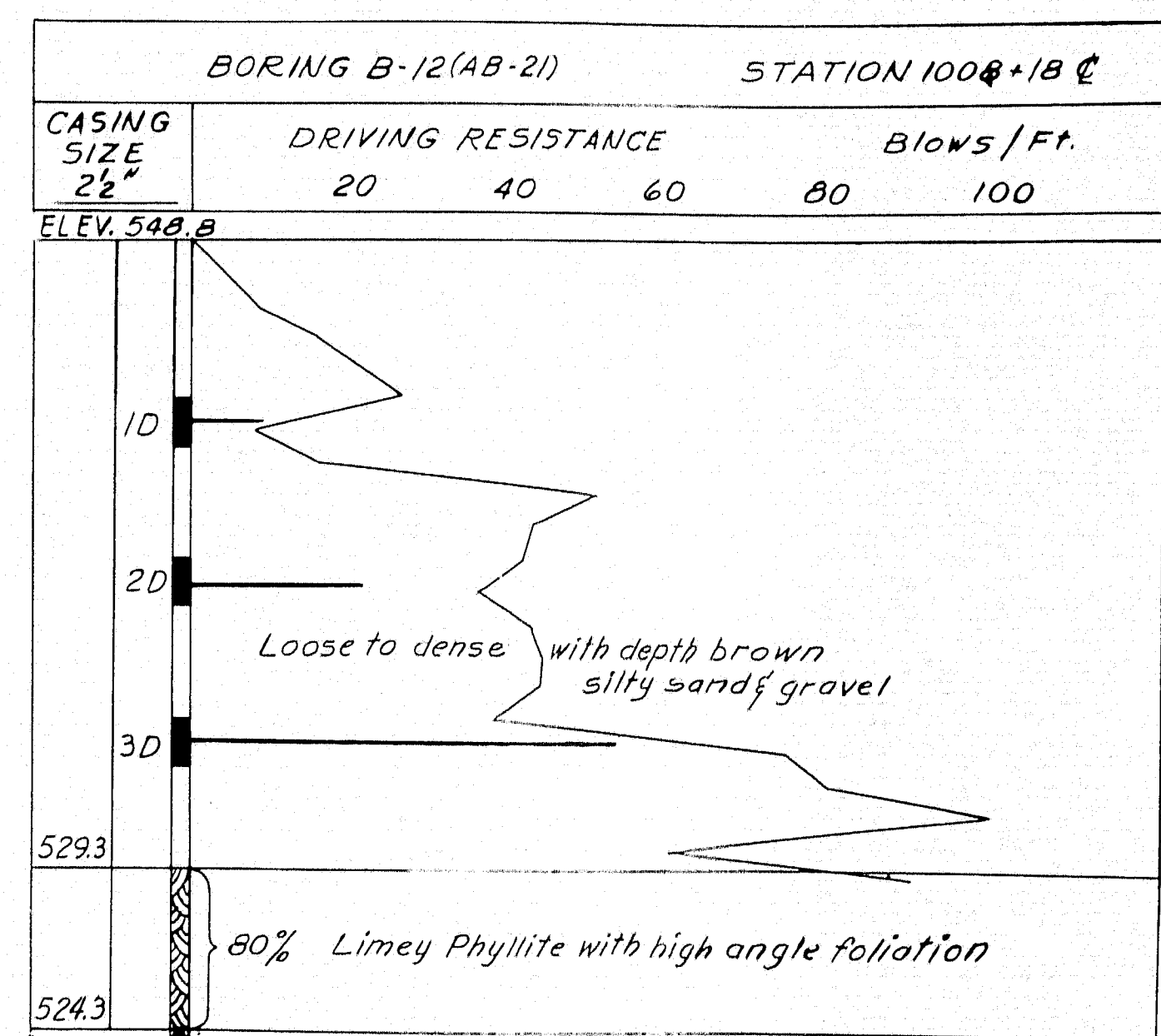
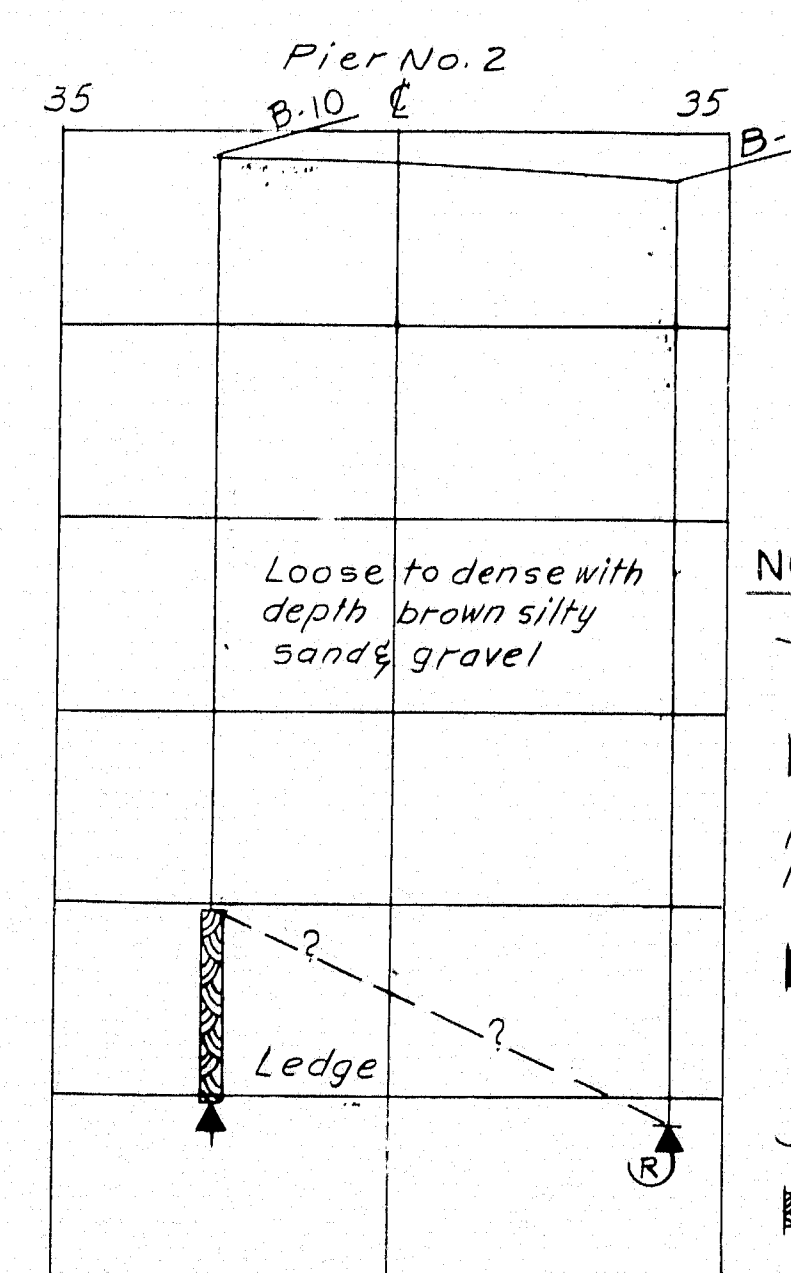
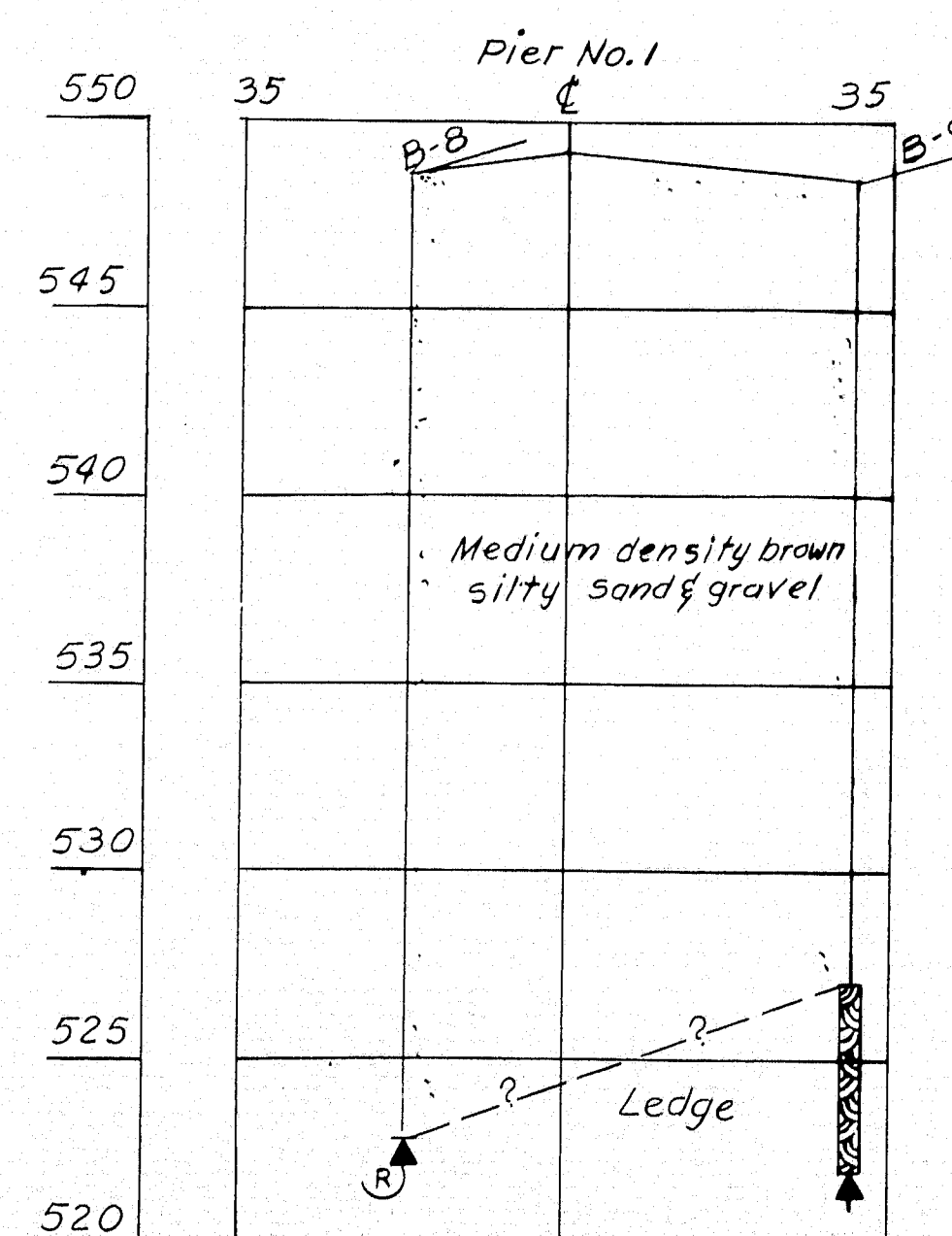
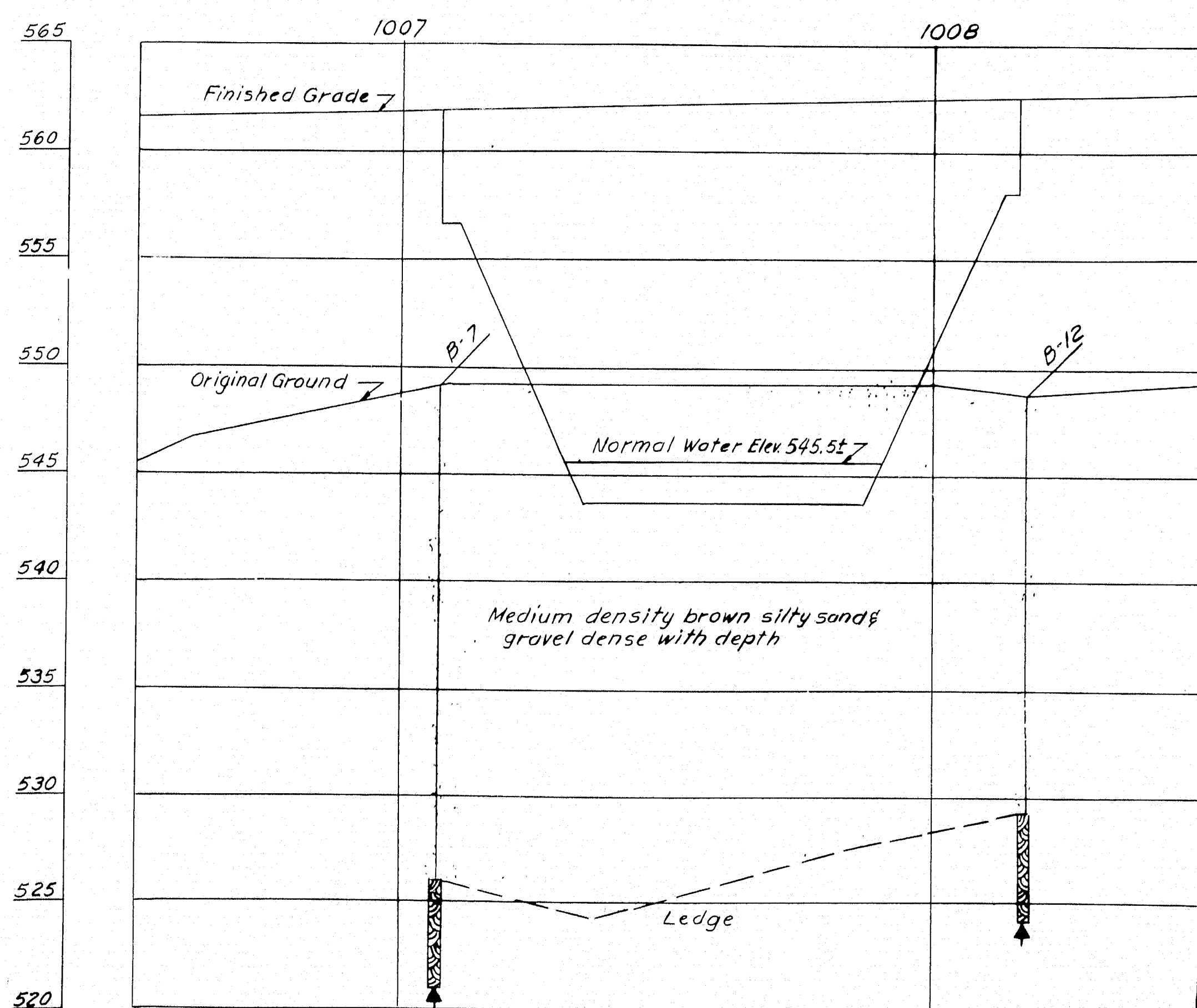
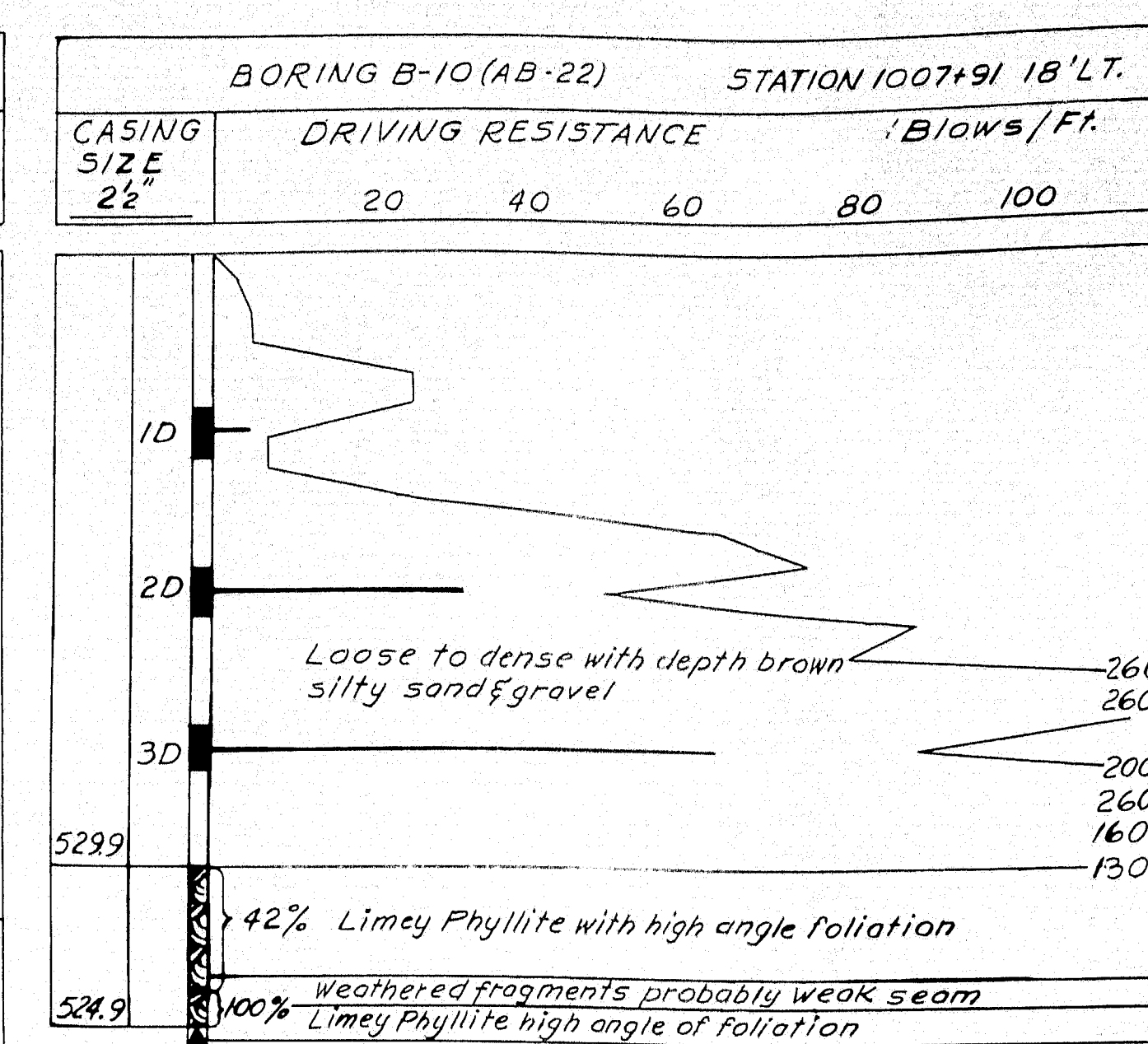
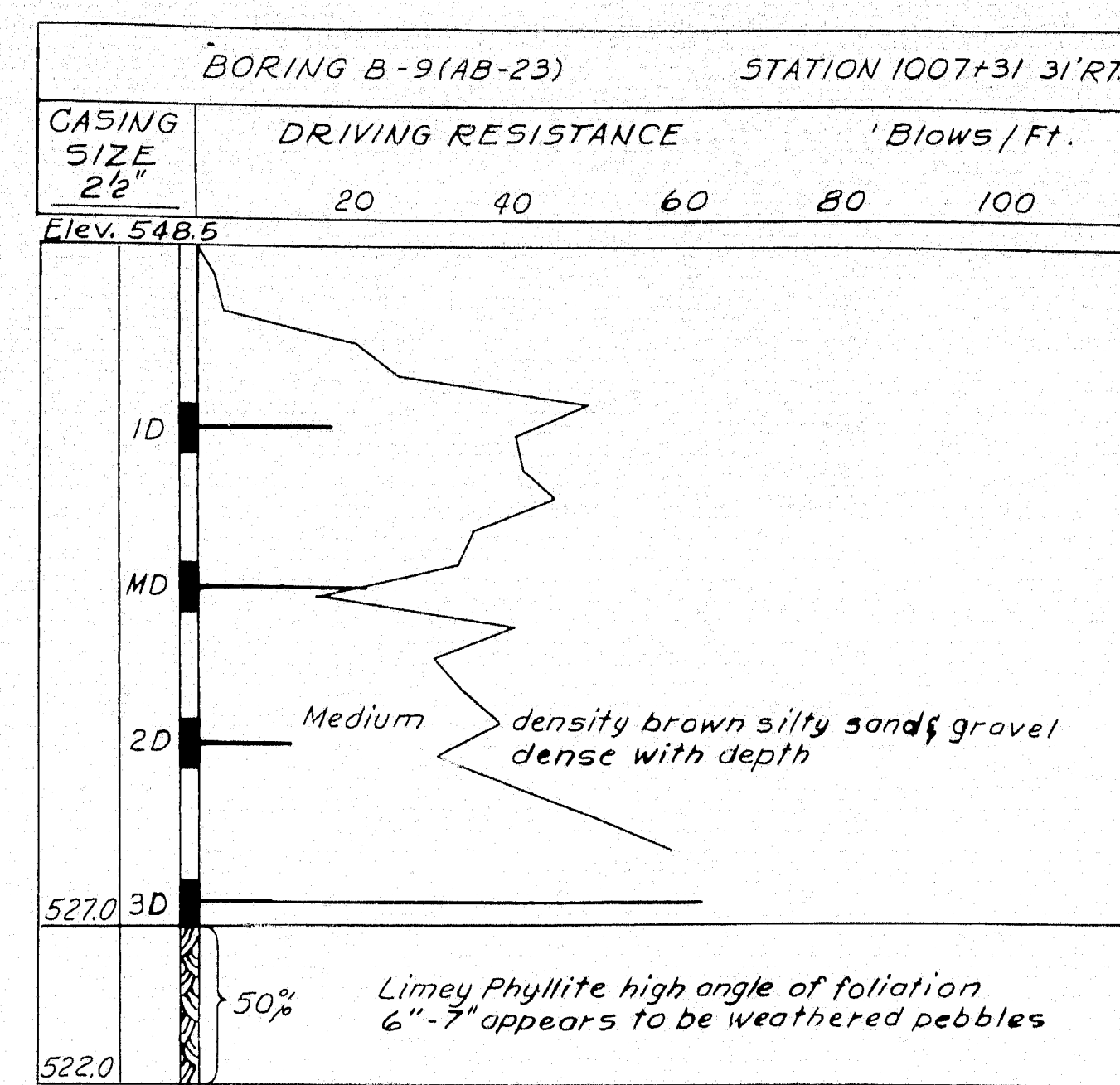
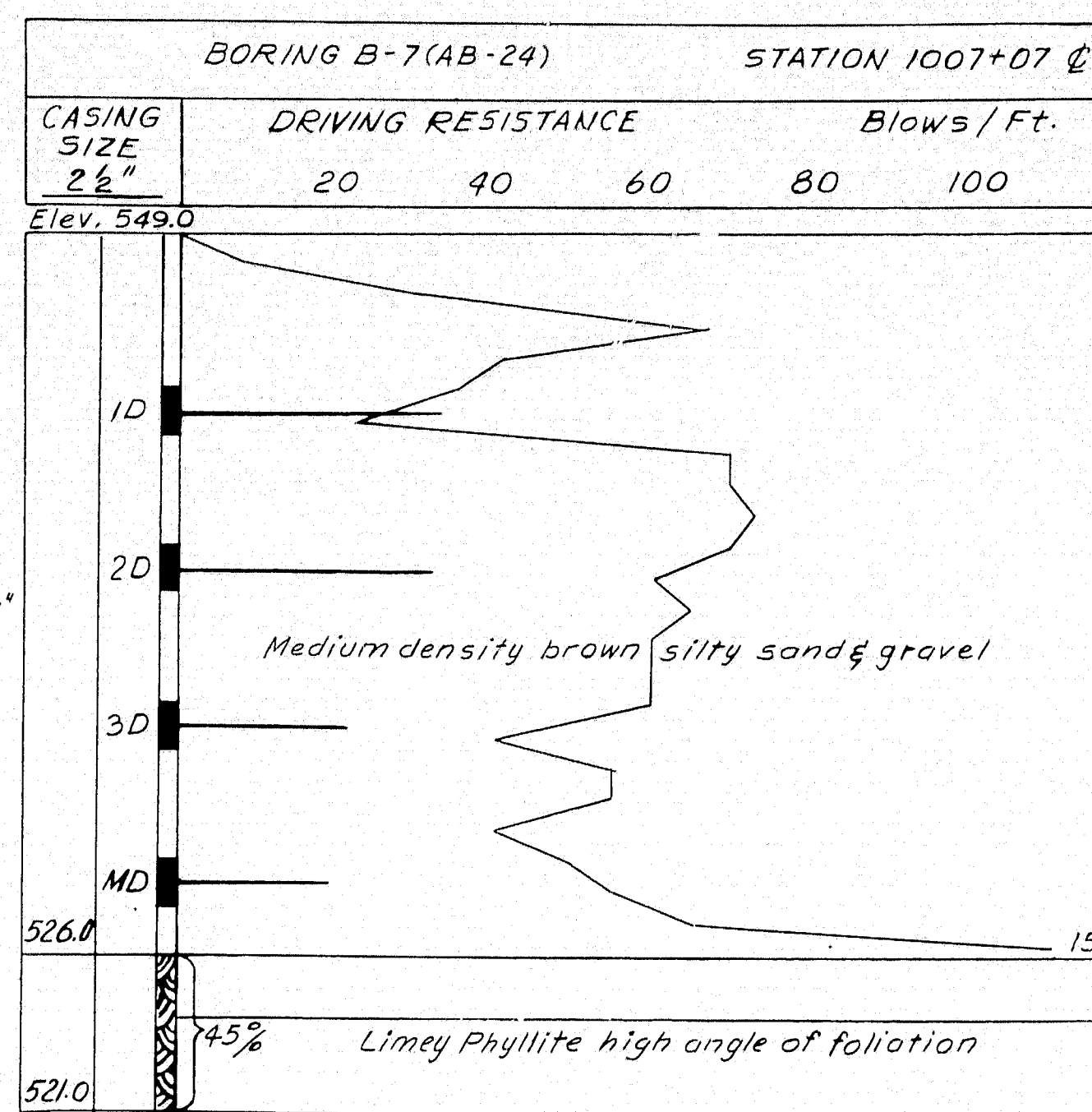
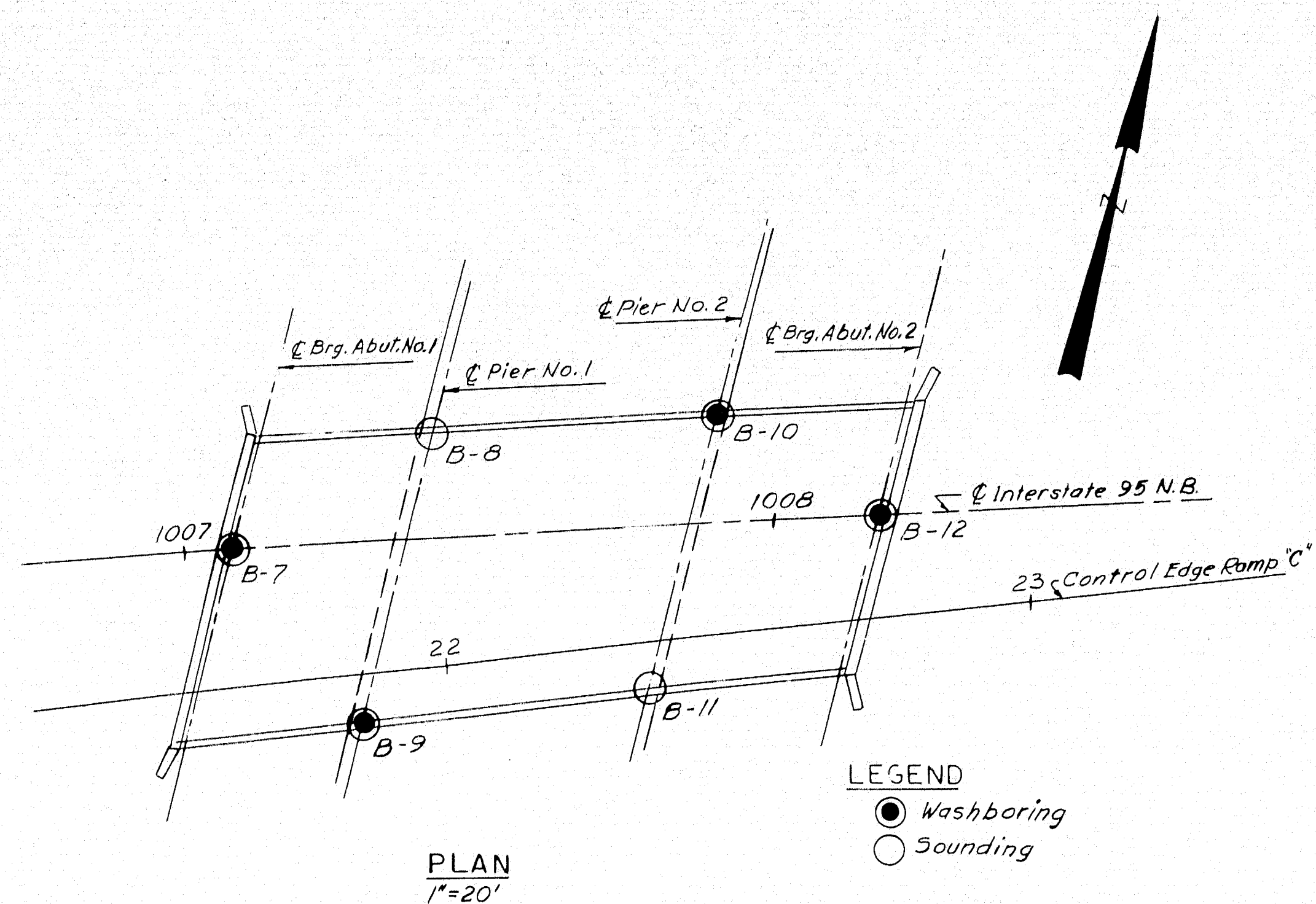
DESIGN - TRACE - CHECK - V.A.V. DETAIL - R.F. BRIDGE NO. SURVEY - PLOT -

STATE HIGHWAY COMMISSION
BRIDGE DIVISION

INTERSTATE 95 S.B.
OVER
RELOCATED EAST BRANCH
MATTAWAMKEAG RIVER
IN THE TOWN OF
OAKFIELD
ARROOSTOOK COUNTY
FOUNDATION SURVEY

SHEET 2 OF 16 AUGUSTA, MAINE FEBRUARY 1965
DYER BROOK OAKFIELD (12)

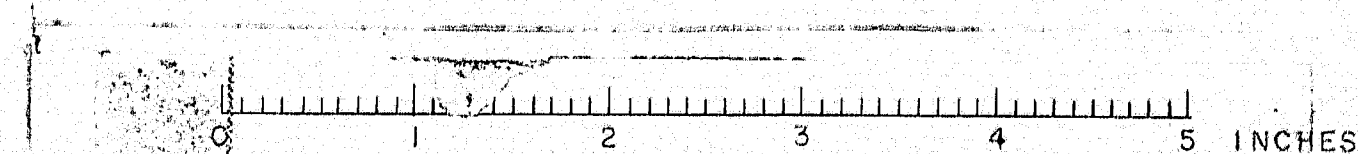




- NOTES:
- Number of blows required to drive extra heavy casing one foot with 400 ft. lbs. of energy per blow.
 - Location of sample or sample attempt.
 - 10 S.H. Sampler #1290's
 - MD unsuccessful sample attempt and type of sampler
 - Number of blows required to drive spoon or tubing one foot with 350 ft. lbs. of energy per blow.
 - Bottom of boring (may not be bottom of soil strata)
 - Refusal of drill rods or casing (may not be ledge)
 - 7% Locations cored by diamond bit and per cent recovery of rock.

DESIGN- TRACE- CHECK-V.A.V.	DETAIL-R.F.	BRIDGE NO. SURVEY- PLOT-
STATE HIGHWAY COMMISSION BRIDGE DIVISION		
INTERSTATE 95 NB OVER		
RELOCATED EAST BRANCH MATTAWAMKEAG RIVER		
IN THE TOWN OF OAKFIELD		
ARROOSTOOK COUNTY FOUNDATION SURVEY		
SHEET 3 OF 16 AUGUSTA, MAINE FEBRUARY 1965		

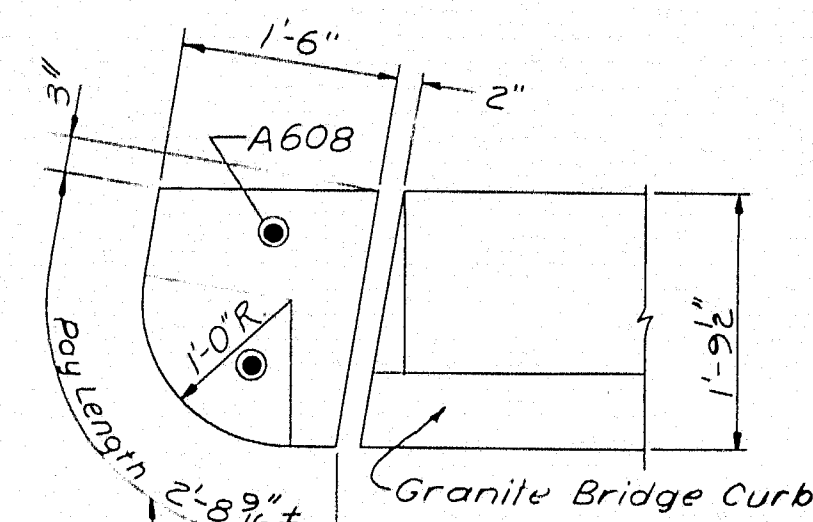
M-2257 DYER BROOK OAKFIELD (12)



NOTE:

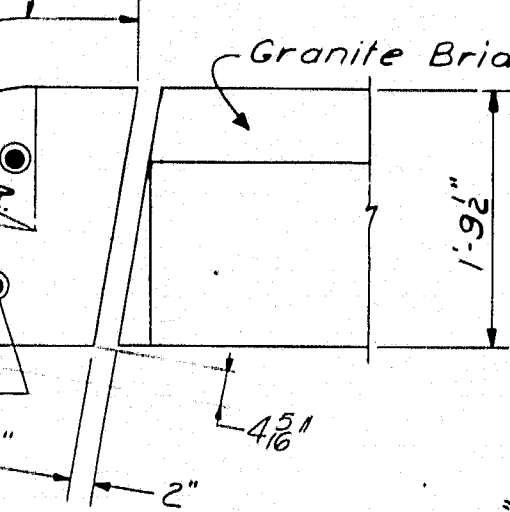
Cover the vertical construction joints on the backside with 2 layers of heavy roofing 10" wide. Bond the layers together and to the concrete with a suitable grade of roofing cement. Recess the vertical areas to be covered 1/4". Paint vertical construction joints with a suitable grade of asphalt paint to break bond.

SECTION D-D
2" = 1'-0"



DETAIL "A"
3" = 1'-0"

Paid for as circular curb. Item 901-25



DETAIL "B"
3" = 1'-0"

NOTES:
1. Grout A608 bars into 14" holes in stone prior to setting stone on backwall. Drill 14" holes in backwall to suit A608 bars.
2. Payment for drilling for and grouting of A608 bars to be included in the price for Item 705-14, Reinforcing Steel, Placing.
3. Granite blocks shall be placed in position after or at the same time as curb on bridge is positioned.

REAR ELEVATION
4" = 1'-0"

* Finished Grade Elevation

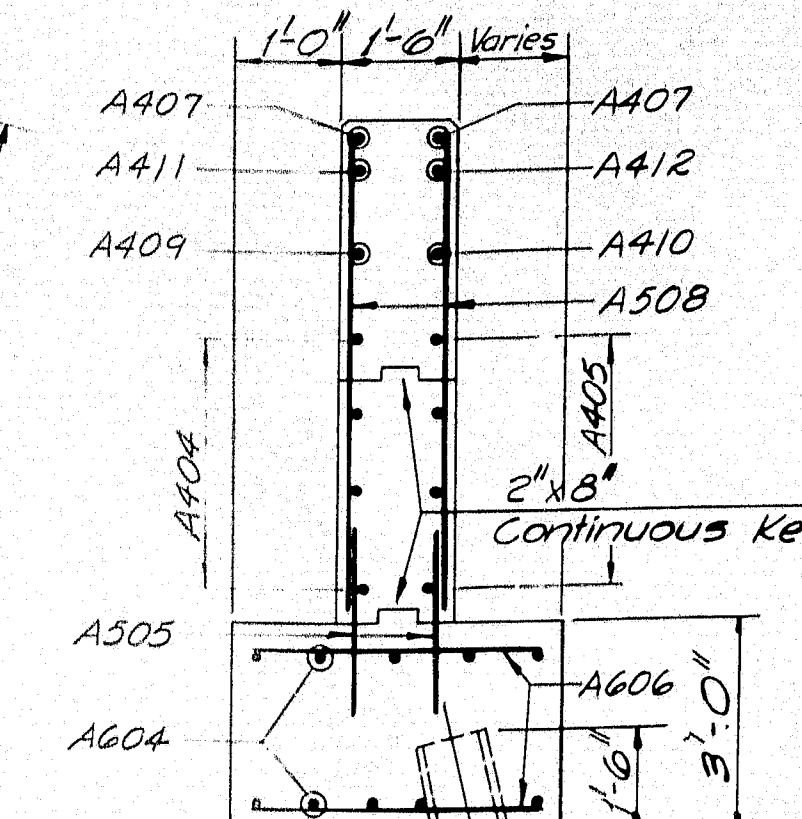
NOTE:

Bar numbers shown in Section A-A and B-B are for Abutment 1 S.B. For Abutment 2 S.B. add 20. For Abutment 1 N.B. add 40. For Abutment 2 N.B. add 60.

FRONT ELEVATION
4" = 1'-0"

* Finished Grade Elevation

SECTION B-B
3" = 1'-0"



SECTION A-A
3" = 1'-0"

GENERAL NOTES:
1. For Approach Slab details see Sheet 5.
2. Paint Bridge Seal, face of Backwall, and 1'-0" below top of Slope Protection on face and ends of Breast Wall with Gray Epoxy Resin Surface Sealant.
3. Dress bearing areas 1" larger all around than the masonry plates to exact elevations shown.
4. Reinforcing steel to have 1" minimum cover unless otherwise shown.
5. Place reinforcing to clear Anchor Bolts.
@ n.f. denotes rear face, f.f. denotes far face, & f.f. denotes each face.

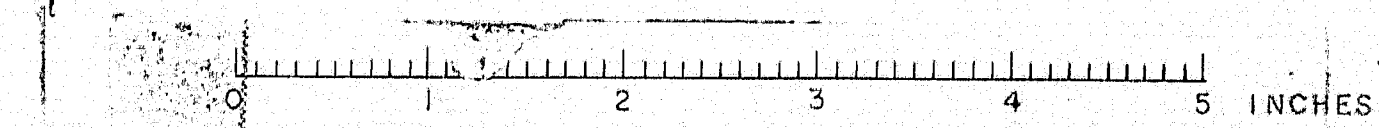
PILE NOTES:

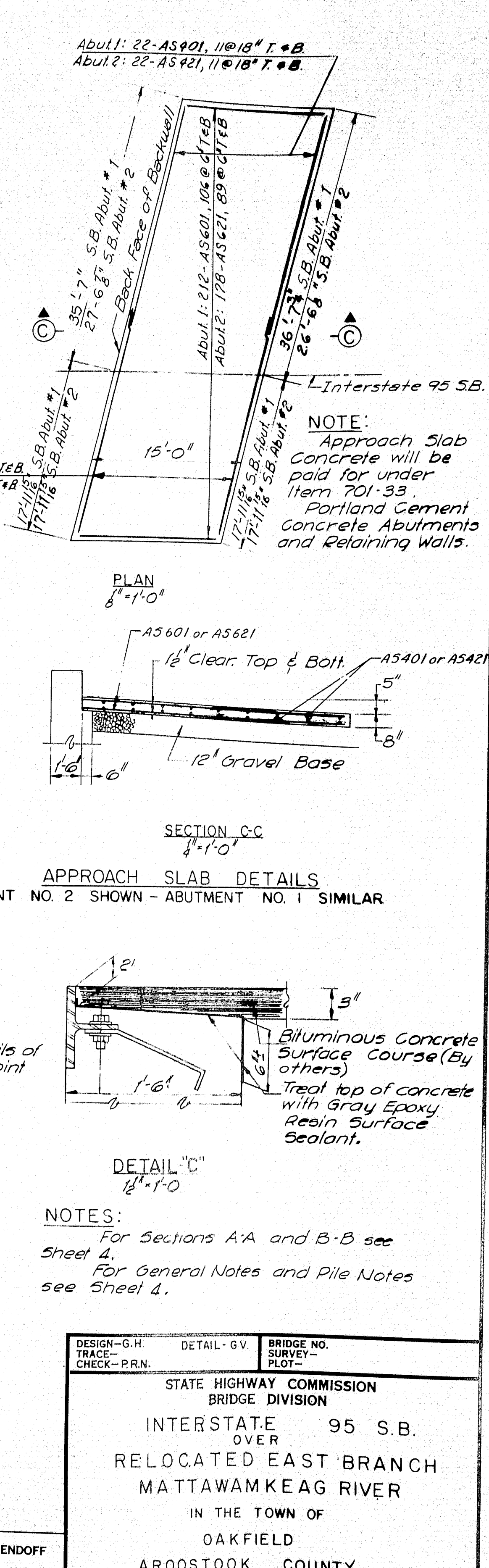
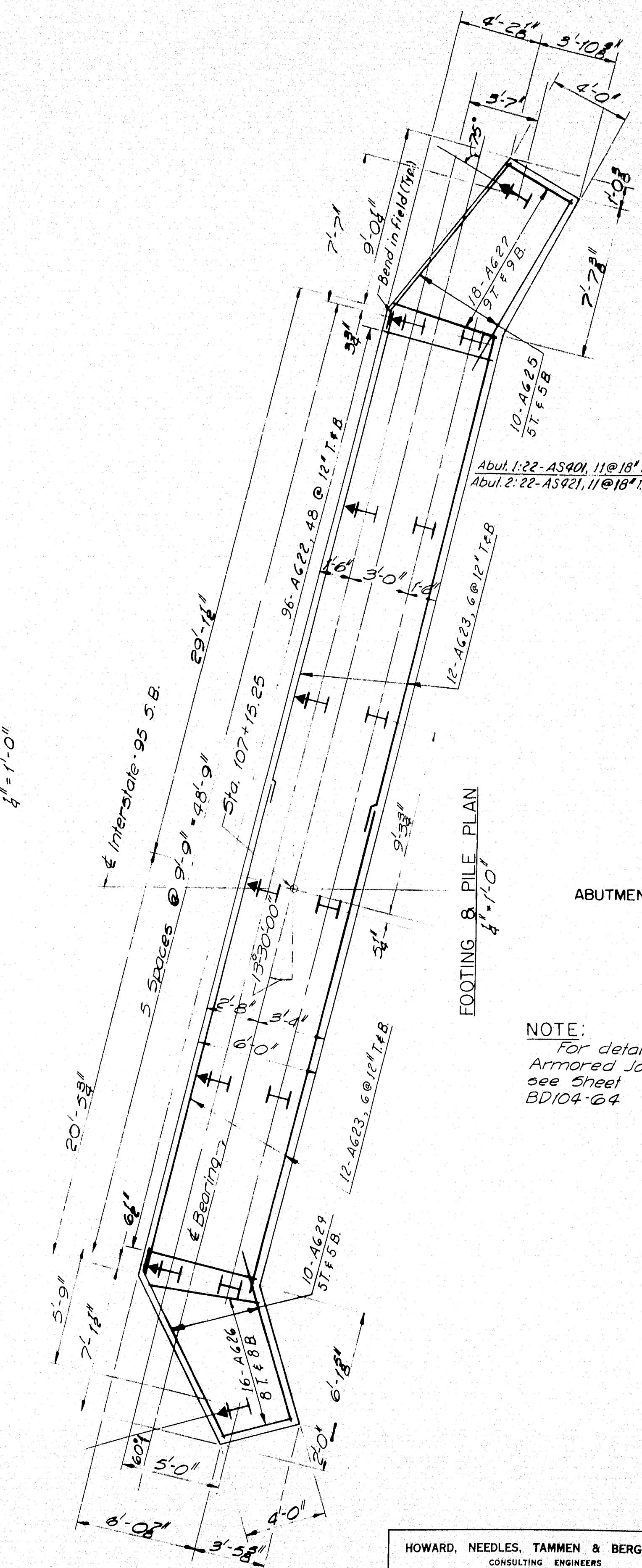
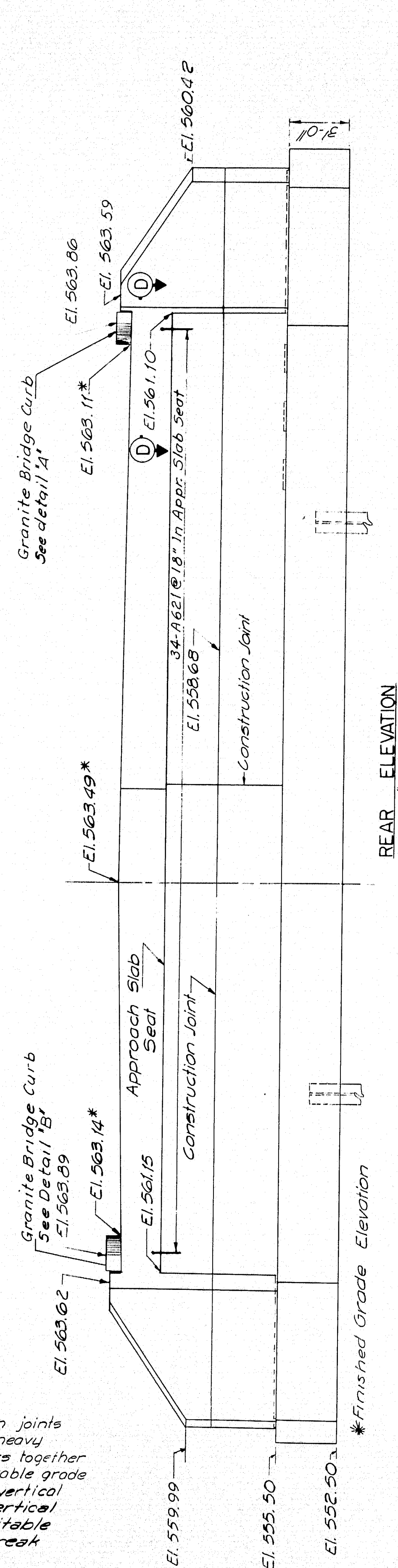
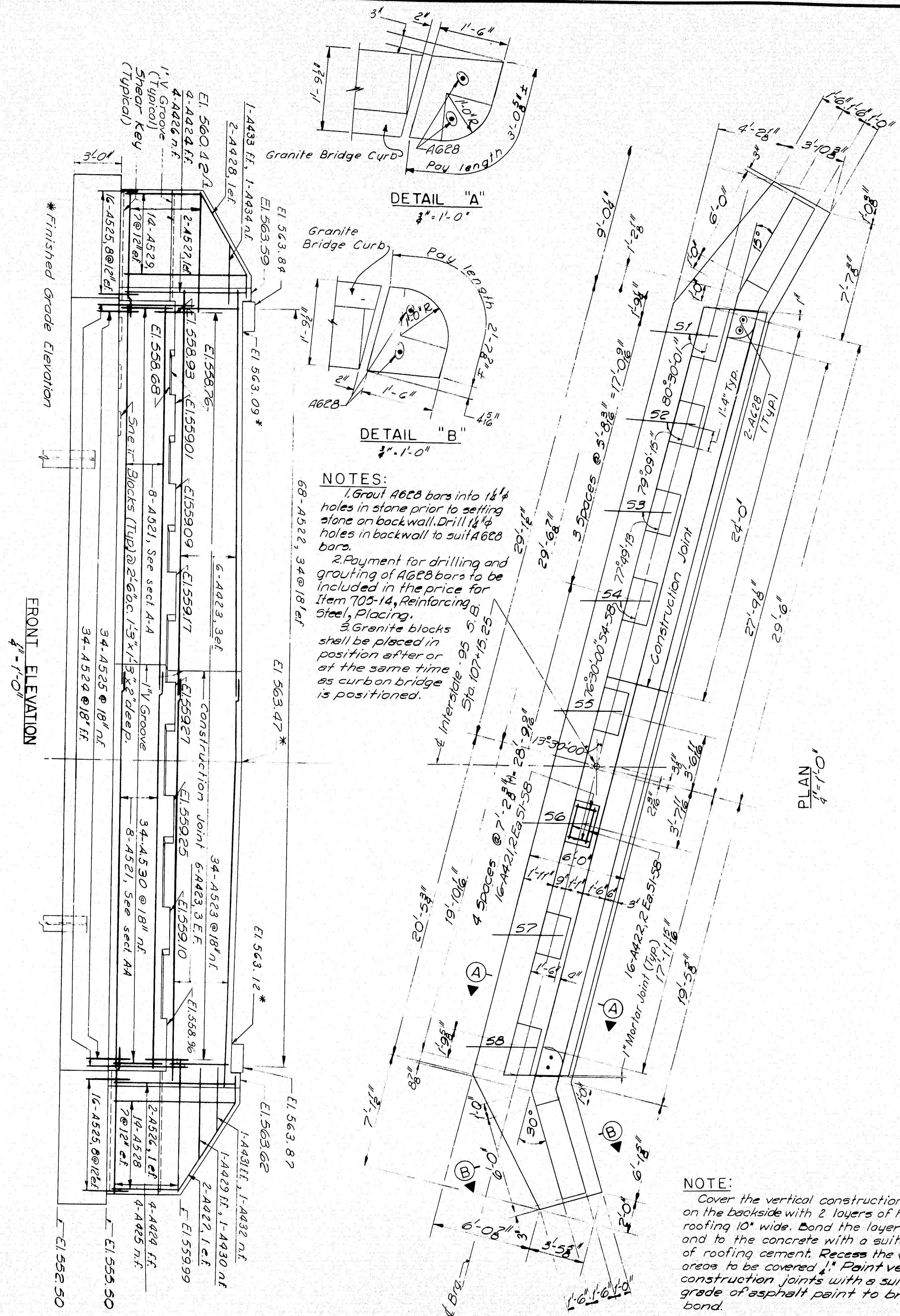
I Indicates Vertical Piles
T Indicates Battered Piles Battered 3:12 in direction of arrow.
All Piles 108 P42 with 37 Ton capacity.
Estimated pile length 34 ft. Abut. 142. Piles to be driven to ledge or practical refusal to develop end bearing.

DESIGN - G.H. DETAIL - G.V. TRACE - CHECK - S.M.	BRIDGE NO. SURVEY - PLOT -
STATE HIGHWAY COMMISSION BRIDGE DIVISION INTERSTATE 95 S.B. OVER RELOCATED EAST BRANCH MATTAWAKEAG RIVER IN THE TOWN OF OAKFIELD ARROOSTOOK COUNTY ABUTMENT NO. 1	
SHEET 4 OF 16 AUGUSTA, MAINE FEBRUARY 1965	

HOWA, D. NEEDLES, TAMMEN & BERGENDOFF
CONSULTING ENGINEERS
NEW YORK BOSTON KANSAS CITY

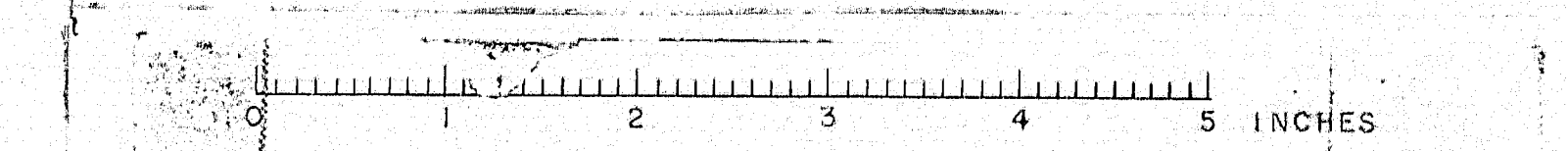
M-2258 DYER BROOK OAKFIELD (12)

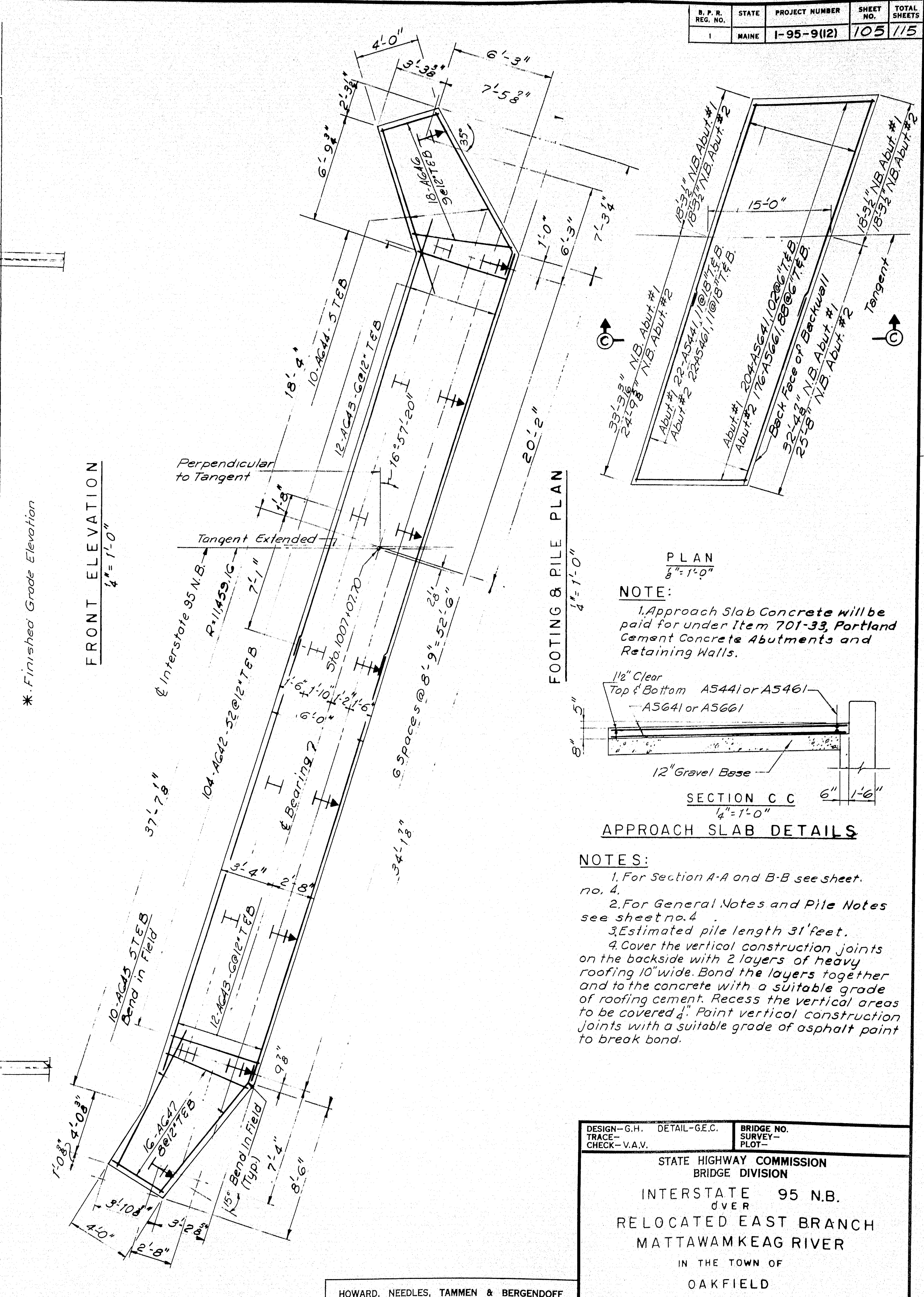
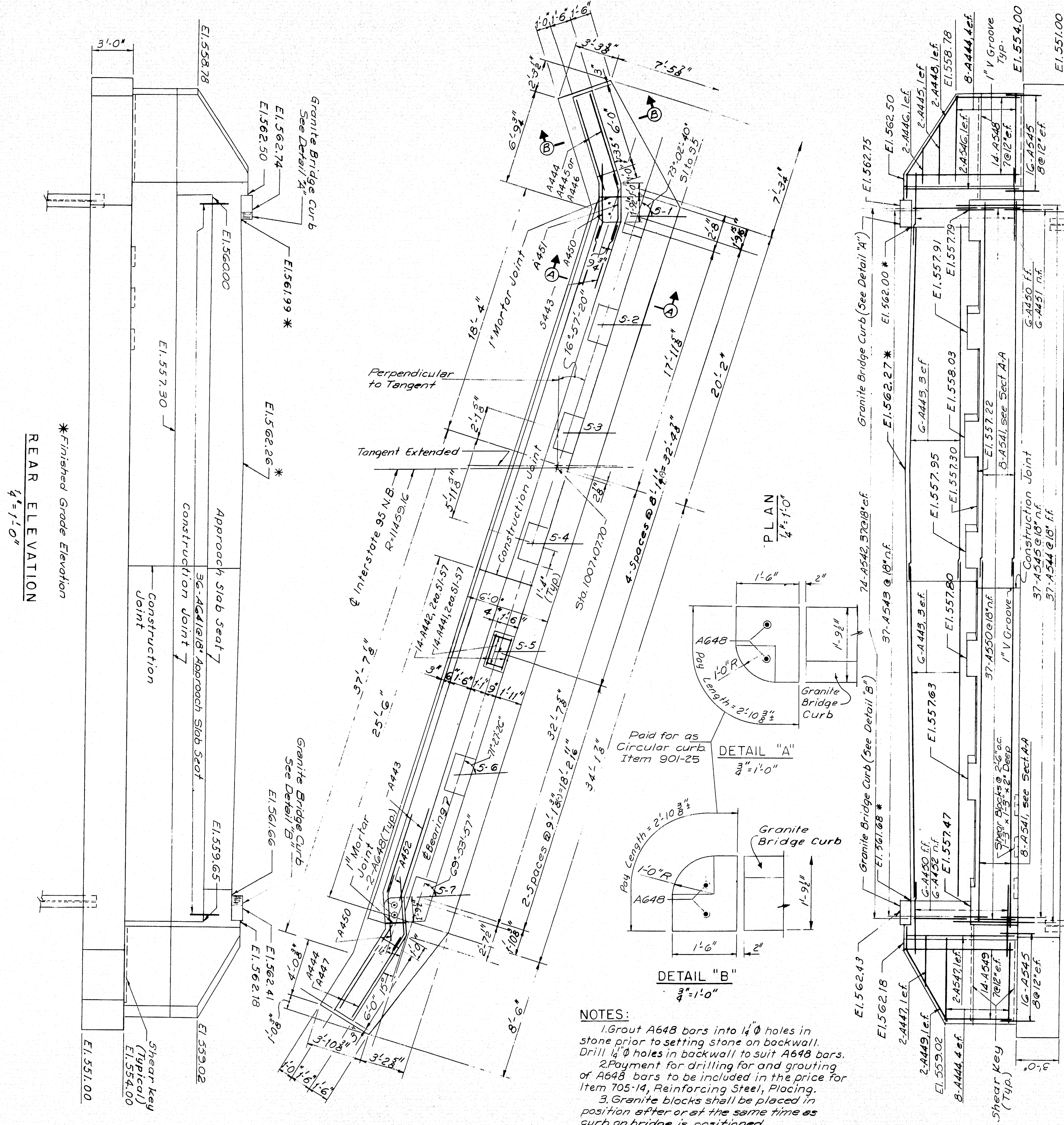




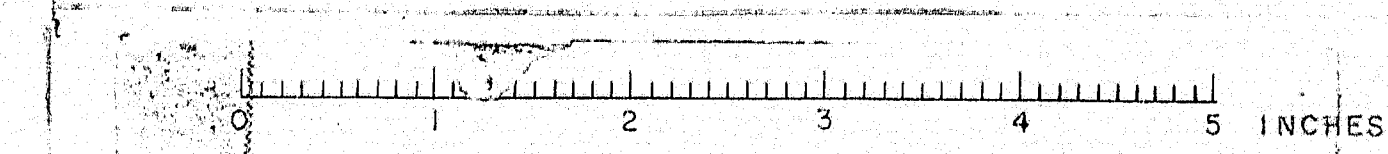
DESIGN - G.H.	DETAIL - G.V.	BRIDGE NO.
TRACE - P.R.N.		SURVEY - PLOT -
STATE HIGHWAY COMMISSION BRIDGE DIVISION		
INTERSTATE 95 S.B. OVER RELOCATED EAST BRANCH MATTAWAMKEAG RIVER IN THE TOWN OF OAKFIELD AROSTOOK COUNTY ABUTMENT NO. 2 & APPROACH SLAB SHEET 5 OF 16 AUGUSTA, MAINE FEBRUARY 1965		
HOWARD, NEEDLES, TAMMEN & BERGENDOFF CONSULTING ENGINEERS NEW YORK BOSTON KANSAS CITY		

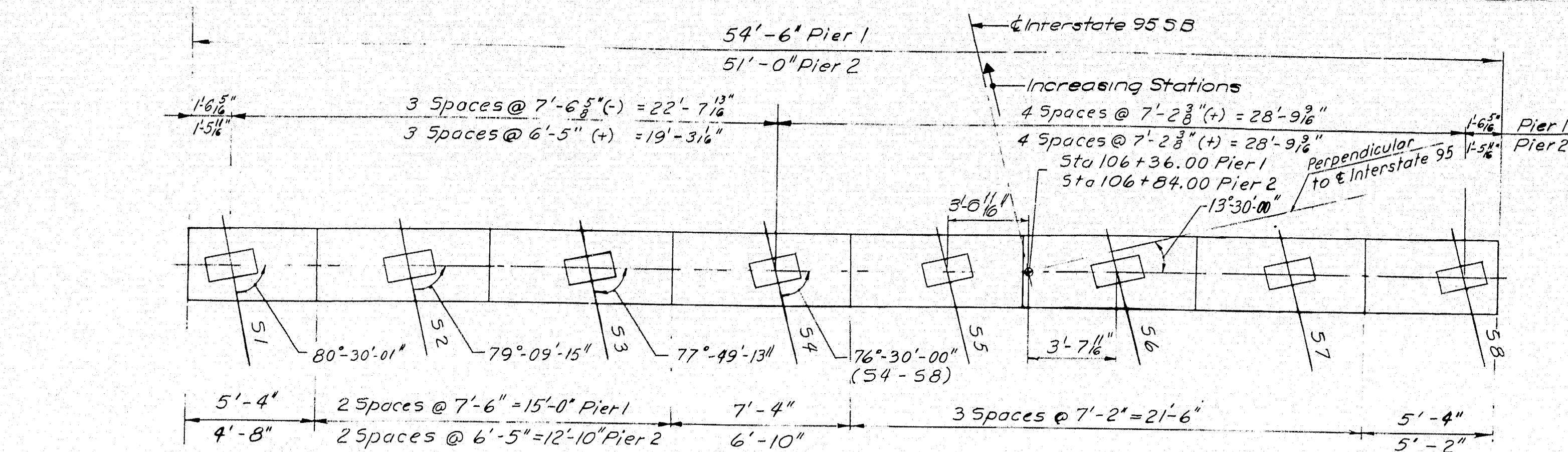
M-2259 DYER BROOK OAKFIELD (12)





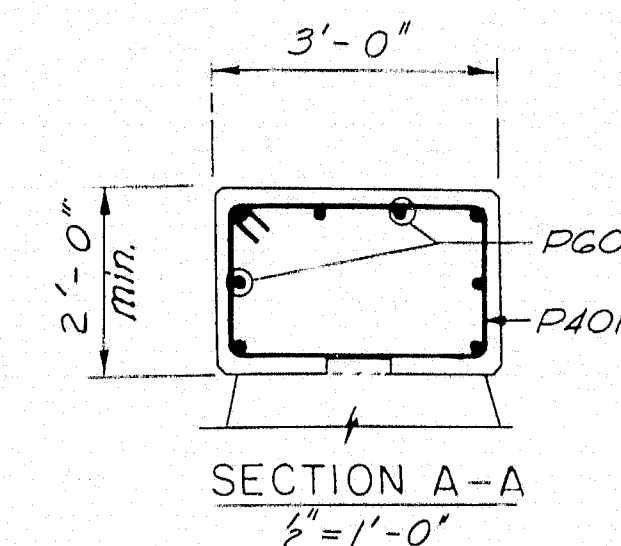
DESIGN - G.H. DETAIL - G.E.C. BRIDGE NO. 95 N.B.
TRACE - V.A.V. SURVEY - PLOT -
STATE HIGHWAY COMMISSION
BRIDGE DIVISION
INTERSTATE 95 N.B.
OVER
RELOCATED EAST BRANCH
MATTAWKEAG RIVER
IN THE TOWN OF
OAKFIELD
ARROOSTOOK COUNTY
ABUTMENT NO. 1 & APPROACH SLAR
SHEET 6 OF 16 AUGUSTA, MAINE FEBRUARY 1965
M-2260 DYER BROOK OAKFIELD (12)



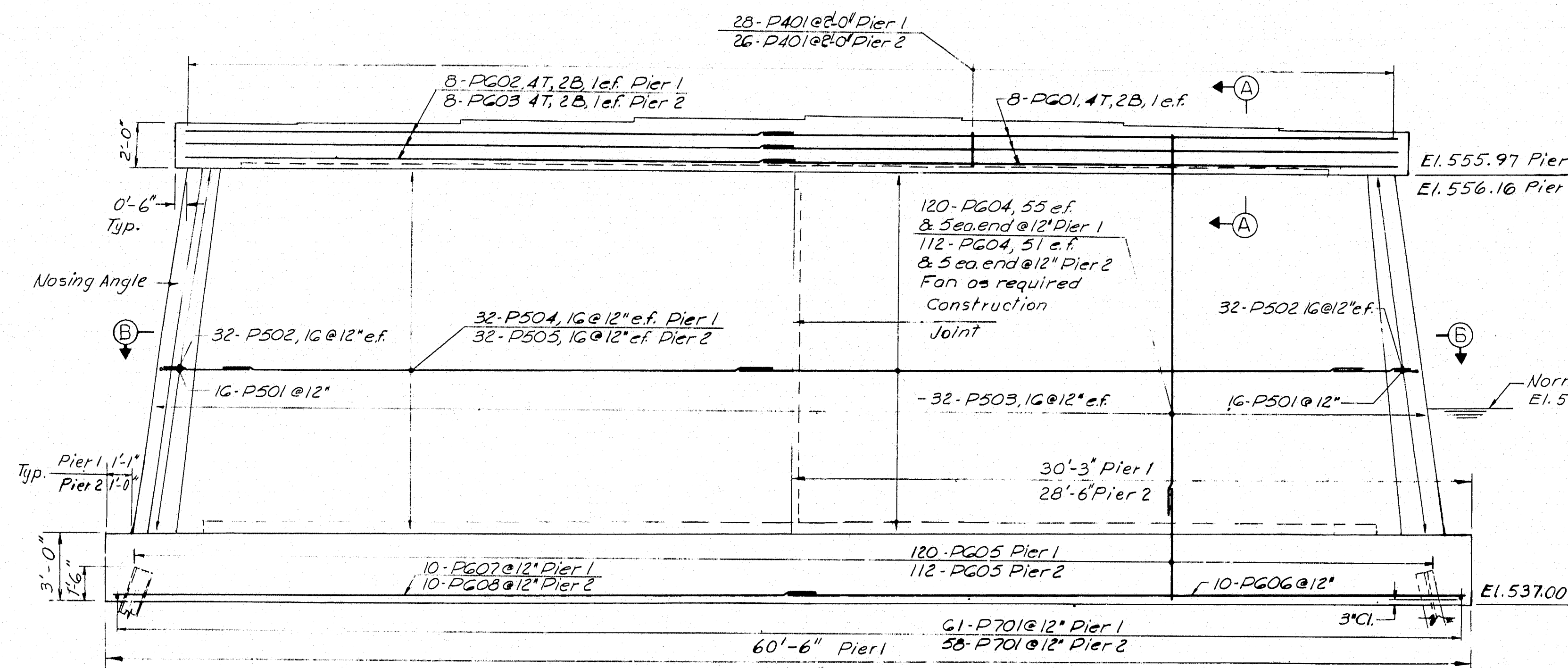


PLAN
1/4" = 1'-0"

BEARING ELEVATIONS		
BEAM	PIER 1	PIER 2
51	557.97	558.16
52	558.08	558.25
53	558.20	558.34
54	558.31	558.44
55	558.42	558.54
56	558.40	558.52
57	558.26	558.38
58	558.13	558.24

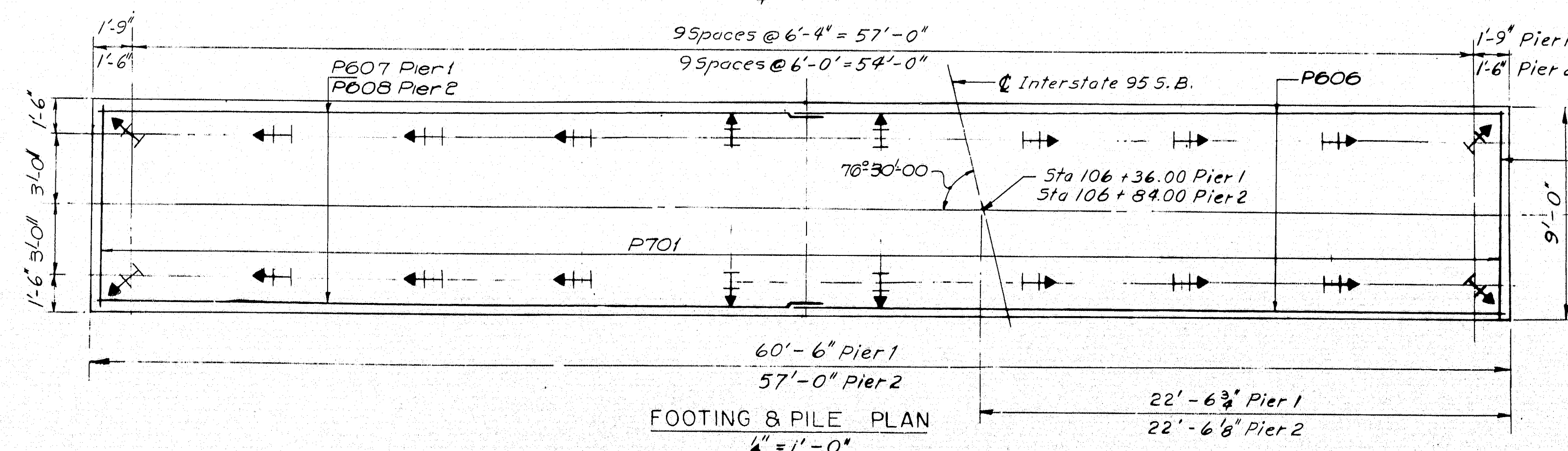


SECTION A-A
1/2" = 1'-0"

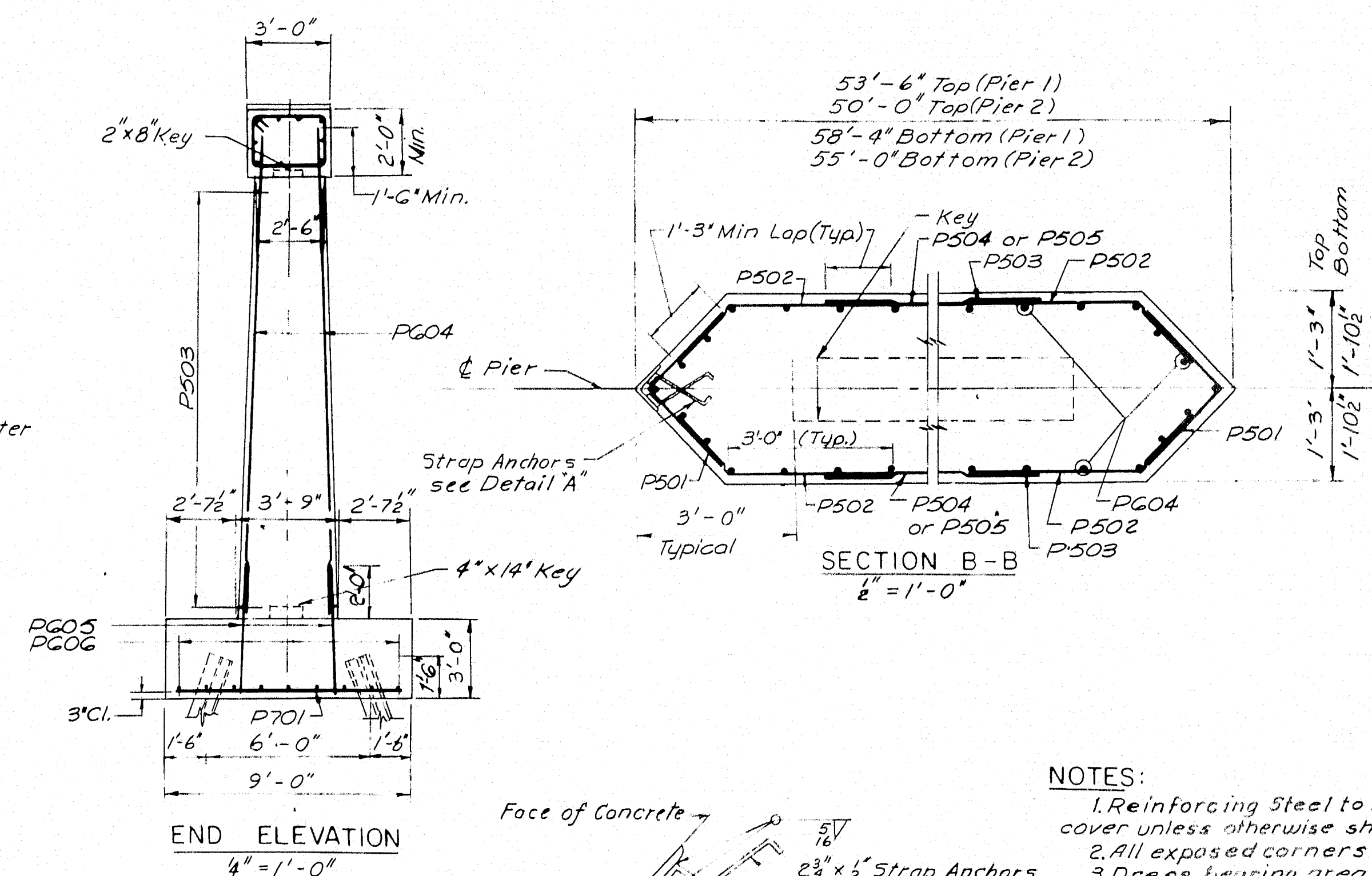


FRONT ELEVATION
1/4" = 1'-0"

NOTE:
Reinforcing shown is typical
for each pier unless otherwise noted.



FOOTING & PILE PLAN
1/4" = 1'-0"



END ELEVATION
1/4" = 1'-0"

NOTES:
1. Reinforcing Steel to have 2" minimum cover unless otherwise shown.
2. All exposed corners to have 1" chamfer.
3. Dress bearing areas 1" larger all around, than masonry plates to exact elevations shown.
4. Place reinforcing to clear anchor bolts.
5. E.F. denotes each face.

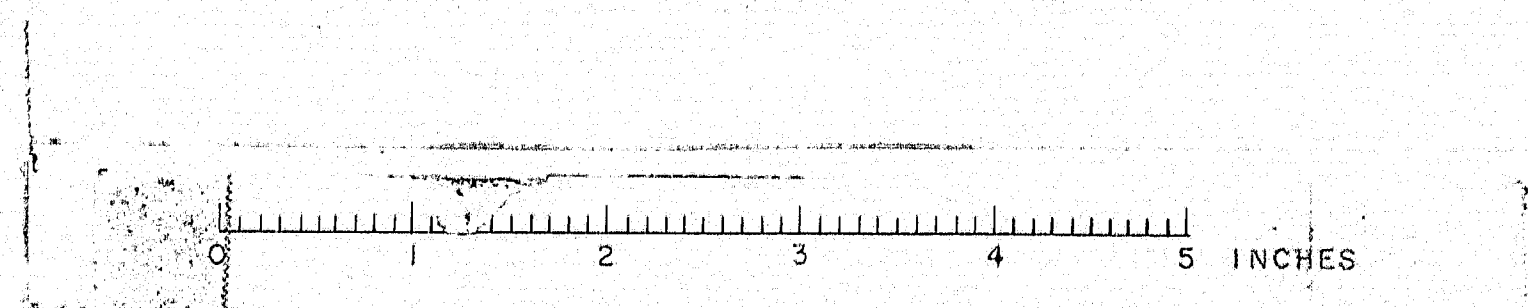
PILE NOTES:
1. Indicates vertical Piles
2. Indicates Batter Pile, battered 3:12 in direction of arrow.
3. All piles 10B P42 Capacity 37 tons
4. Estimated Pile Length: 23 feet
5. Piles to be driven to ledge or practical refusal to develop end bearings.

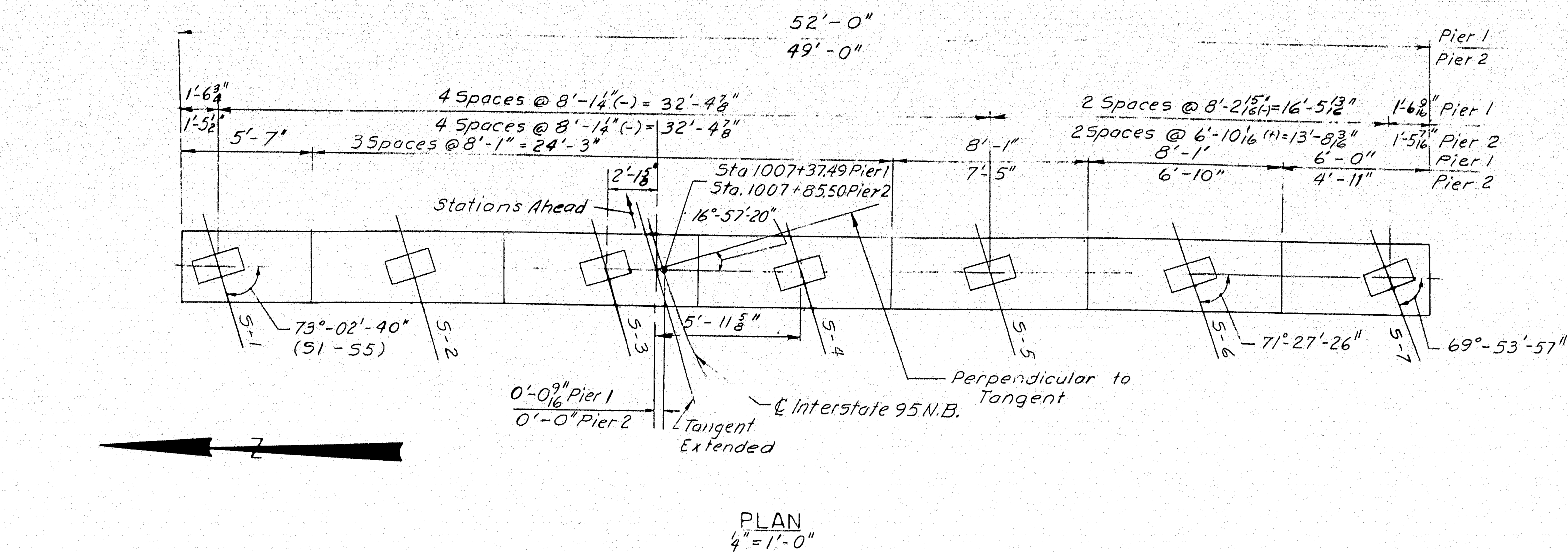
DETAIL A
1/4" = 1'-0"

HOWARD, NEEDLES, TAMMEN & BERGENDOFF
CONSULTING ENGINEERS
NEW YORK BOSTON KANSAS CITY

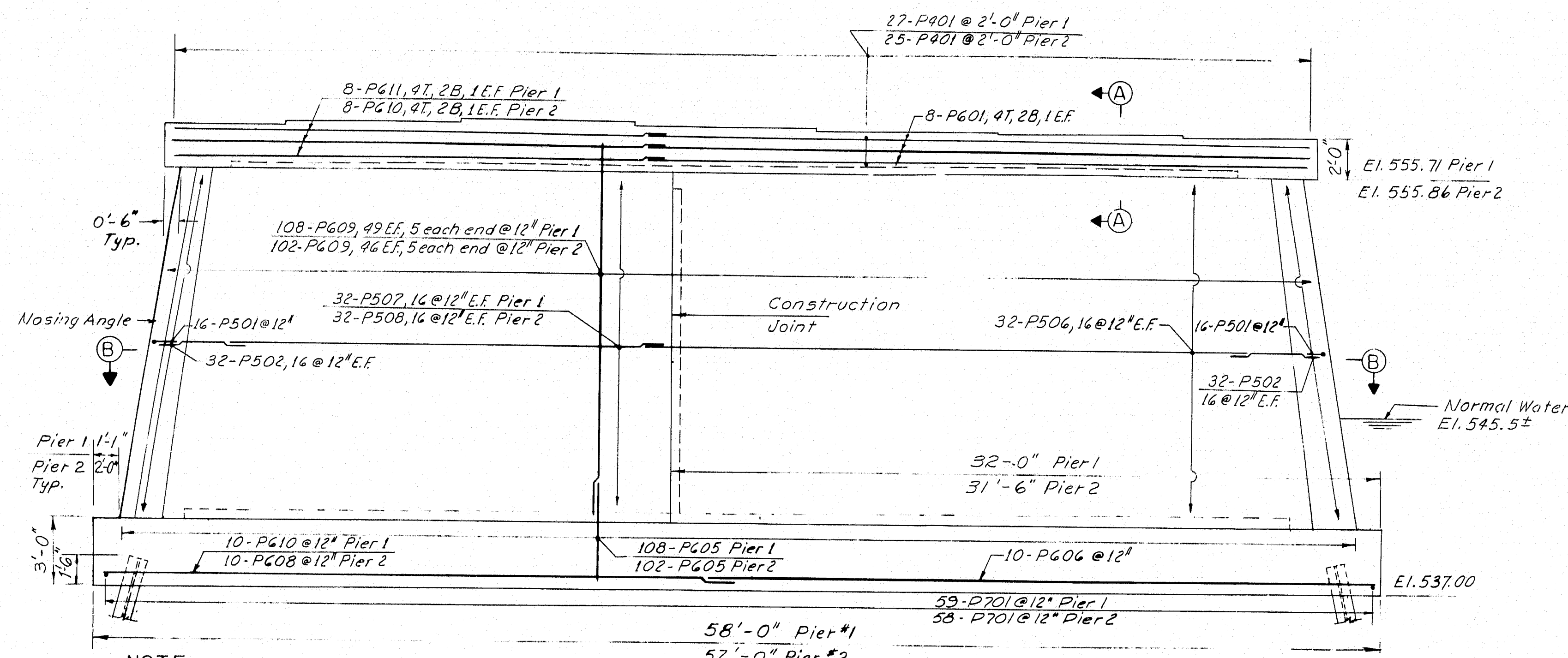
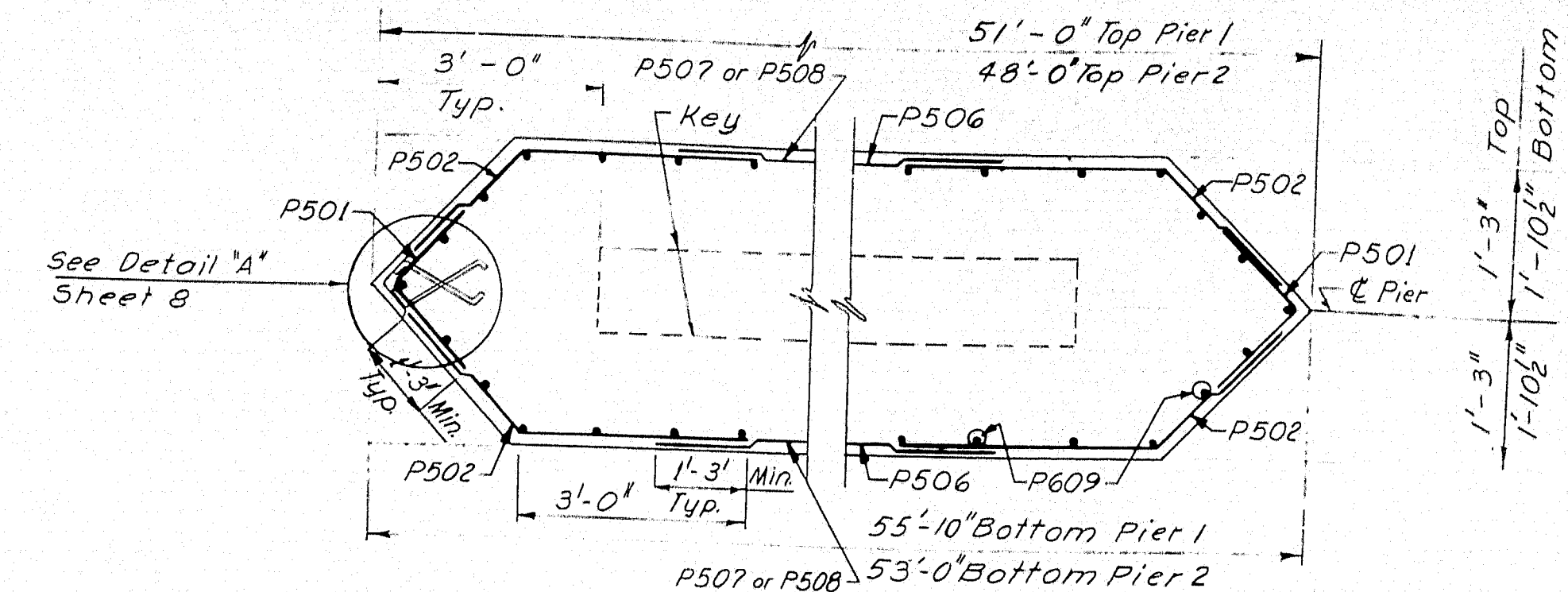
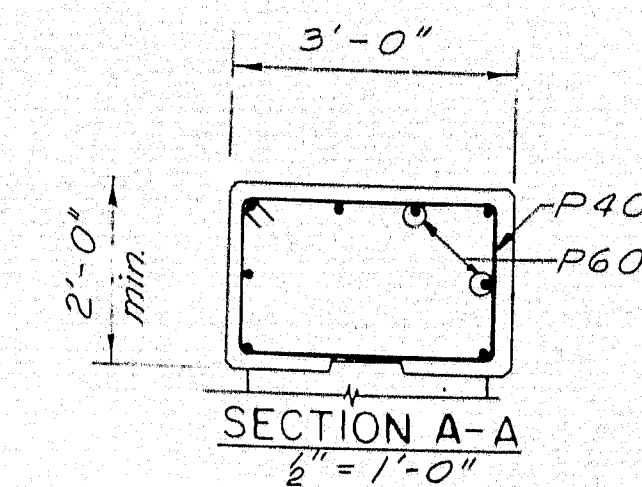
DESIGN - E. F. T. OF 12/11/95
TRACE - P. R. P.
CHECK - P. R. P.
BRIDGE NO. 107
STATE HIGHWAY COMMISSION
BRIDGE DIVISION
INTERSTATE 95 S.B.
OVER
RELOCATED EAST BRANCH
MATTAWAMKEAG RIVER
IN THE TOWN OF
OAKFIELD
AROSTOOK COUNTY
PIERS
SHEET 8 OF 16 AUGUSTA, MAINE FEBRUARY 1965
DYER BROOK OAKFIELD (12)

M-2262

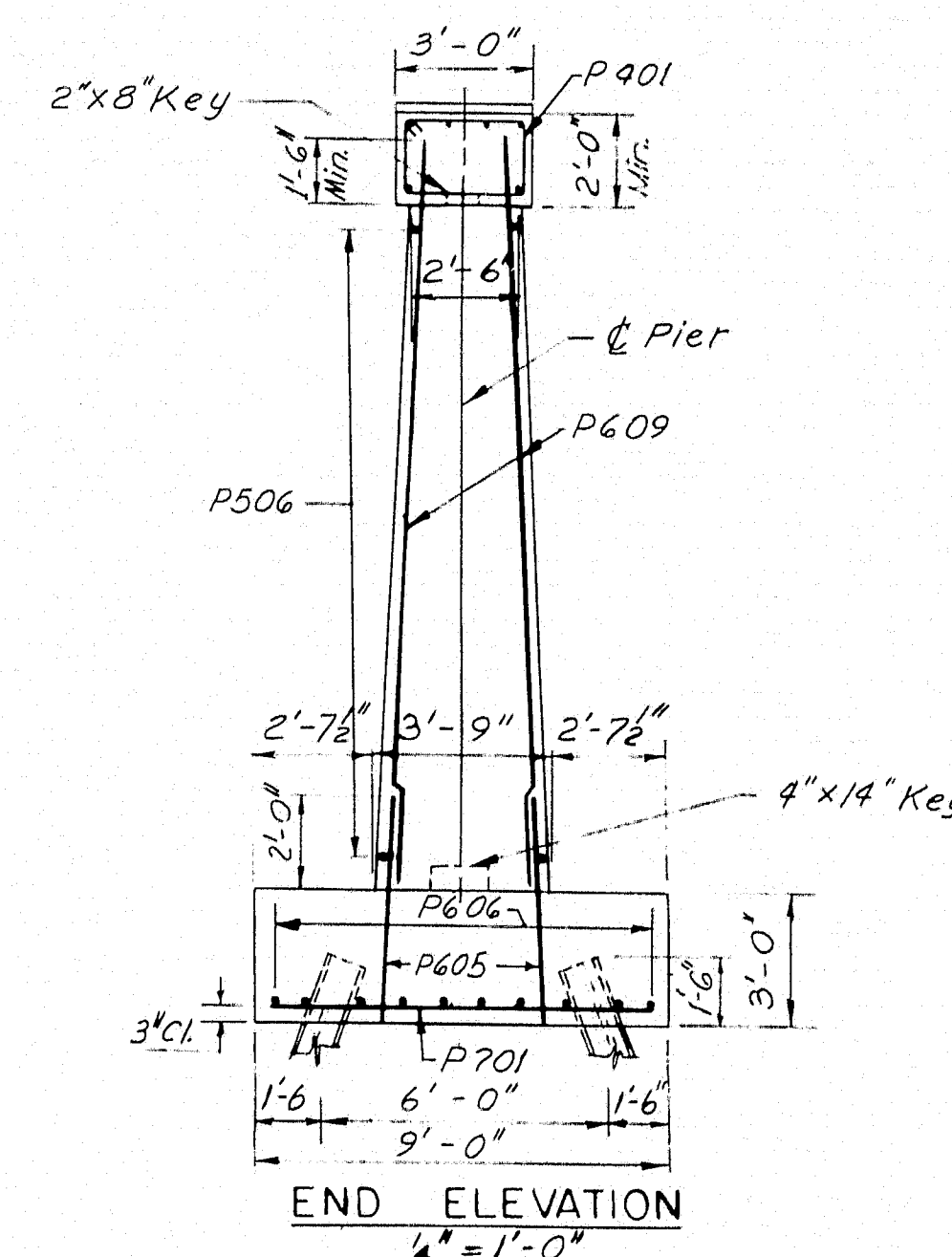
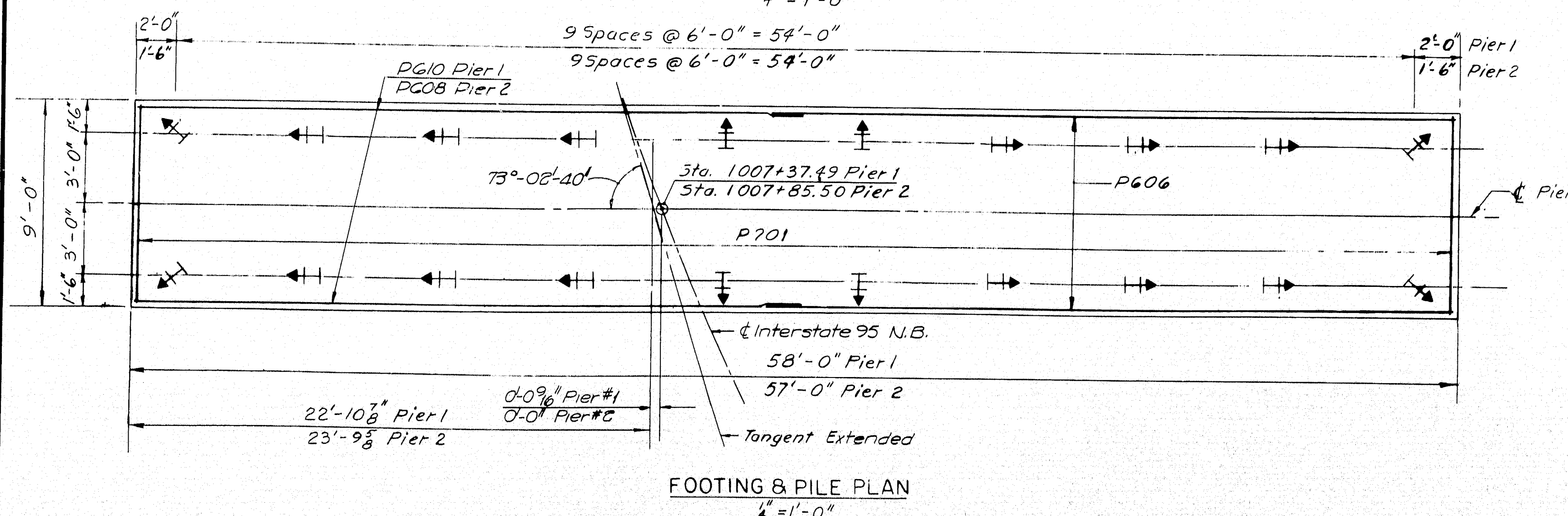




BEAM	PIER 1	PIER 2
S1	558.03	558.16
S2	558.15	558.27
S3	558.26	558.38
S4	558.18	558.29
S5	558.02	558.13
S6	557.87	557.99
S7	557.71	557.86



NOTE:
Reinforcing shown is typical for each pier unless otherwise noted.



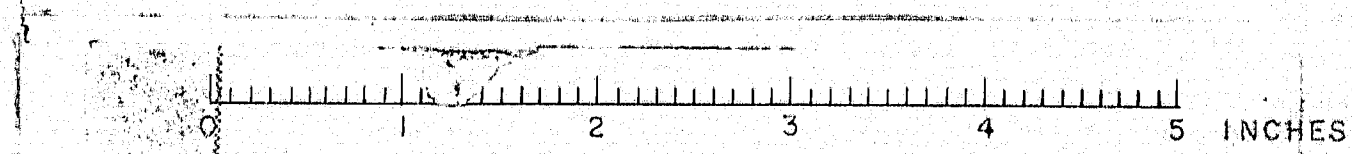
- NOTES:
1. Reinforcing steel to have 2" minimum cover unless otherwise shown.
 2. All exposed corners to have 1" chamfer.
 3. Dress bearing areas 1" larger all around, than masonry plates to exact elevations shown.
 4. Place reinforcing to clear anchor bolts.
 5. E.F. denotes each face.

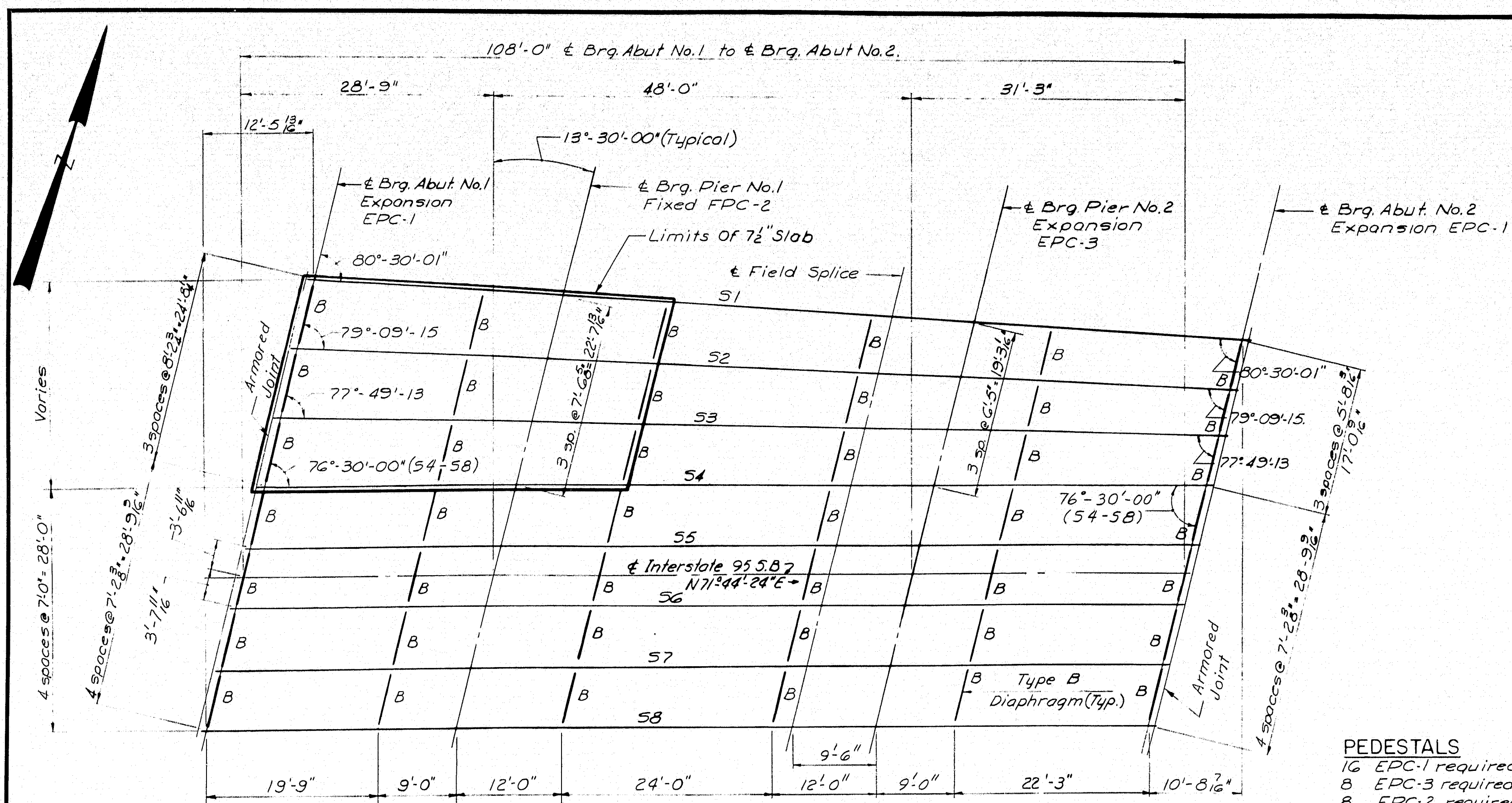
- PILE NOTES:
1. I Indicates vertical piles.
 2. I Indicates Batter Pile, battered 3:12 in direction of arrow.
 3. All piles 10BP42 capacity = 37 Tons.
 4. Estimated Pile Length: 23 feet.
 5. Piles to be driven to ledge or practical refusal to develop end bearing.

DESIGN - E.F.K. DETAIL-R.F.	BRIDGE NO.
TRACE - P.R.N.	SURVEY - PLOT
STATE HIGHWAY COMMISSION	
BRIDGE DIVISION	
INTERSTATE 95 N.B.	
OVER	
RELOCATED EAST BRANCH	
MATTAWAMKEAG RIVER	
IN THE TOWN OF	
OAKFIELD	
AROSTOOK COUNTY	
PIERS	
HOWARD, NEEDLES, TAMMEN & BERGENDOFF CONSULTING ENGINEERS	SHEET 9 OF 16
NEW YORK BOSTON KANSAS CITY	AUGUSTA, MAINE FEBRUARY 1965

M-2263

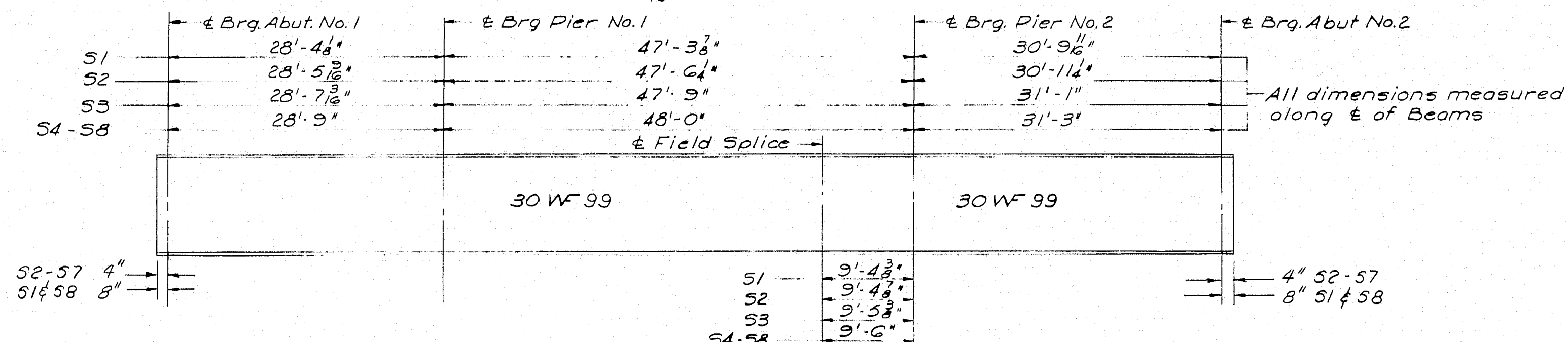
DYER BROOK OAKFIELD (12)





ERECTION DIAGRAM

1" = 10'

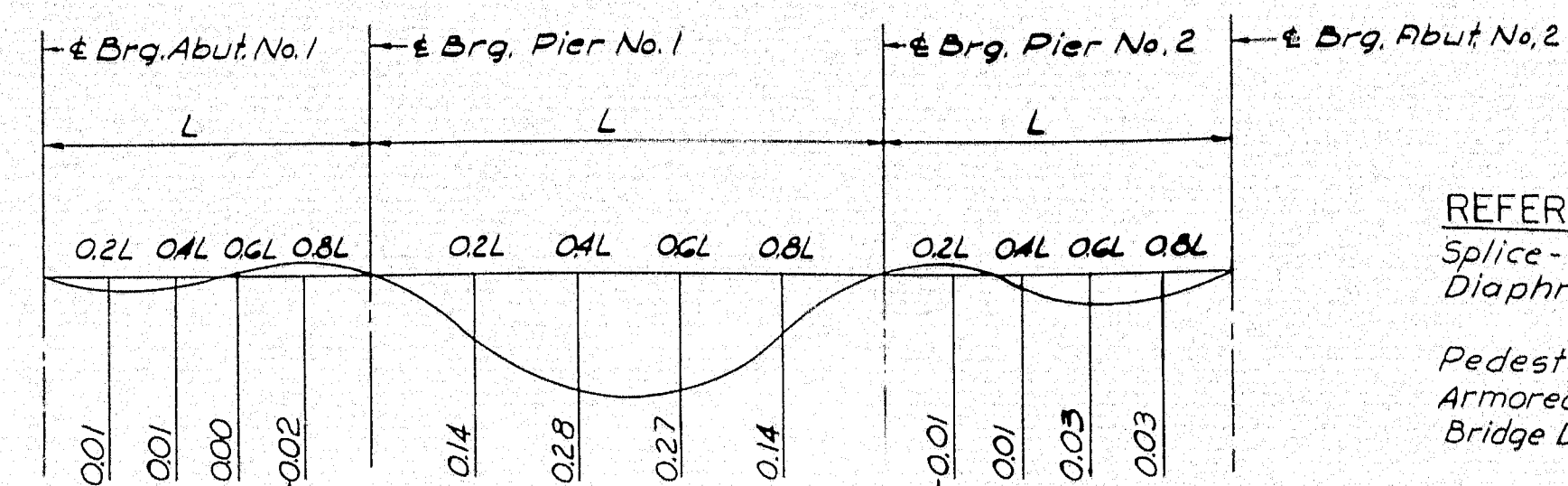


TYPICAL STRINGER ELEVATION

All dimensions horizontal

BOTTOM OF SLAB ELEVATIONS AT BLOCKING POINTS

	Span No. 1	Span No. 2	Span No. 3	Span No. 4
	Span No. 1	Span No. 2	Span No. 3	Span No. 4
Line 1	561.03	561.08	561.14	561.19
Line 2	561.15	561.20	561.25	561.31
Line 3	561.28	561.33	561.37	561.42
Line 4	561.40	561.45	561.49	561.54
Line 5	561.55	561.60	561.64	561.69
Line 6	561.54	561.58	561.63	561.67
Line 7	561.40	561.45	561.49	561.54
Line 8	561.27	561.31	561.36	561.40
S1	5'-8 3/8"	11'-5 1/4"	17'-1 1/2"	22'-10 1/2"
S2	5'-8 3/8"	11'-4 1/2"	17'-0 1/2"	22'-9 1/2"
S3	5'-8 3/8"	11'-4 1/2"	17'-0 1/2"	22'-9 1/2"



DEAD LOAD DEFLECTION DIAGRAM

ALL DEFLECTIONS IN INCHES

NOTE:
No Shop camber required.
Natural mill camber to be placed up.

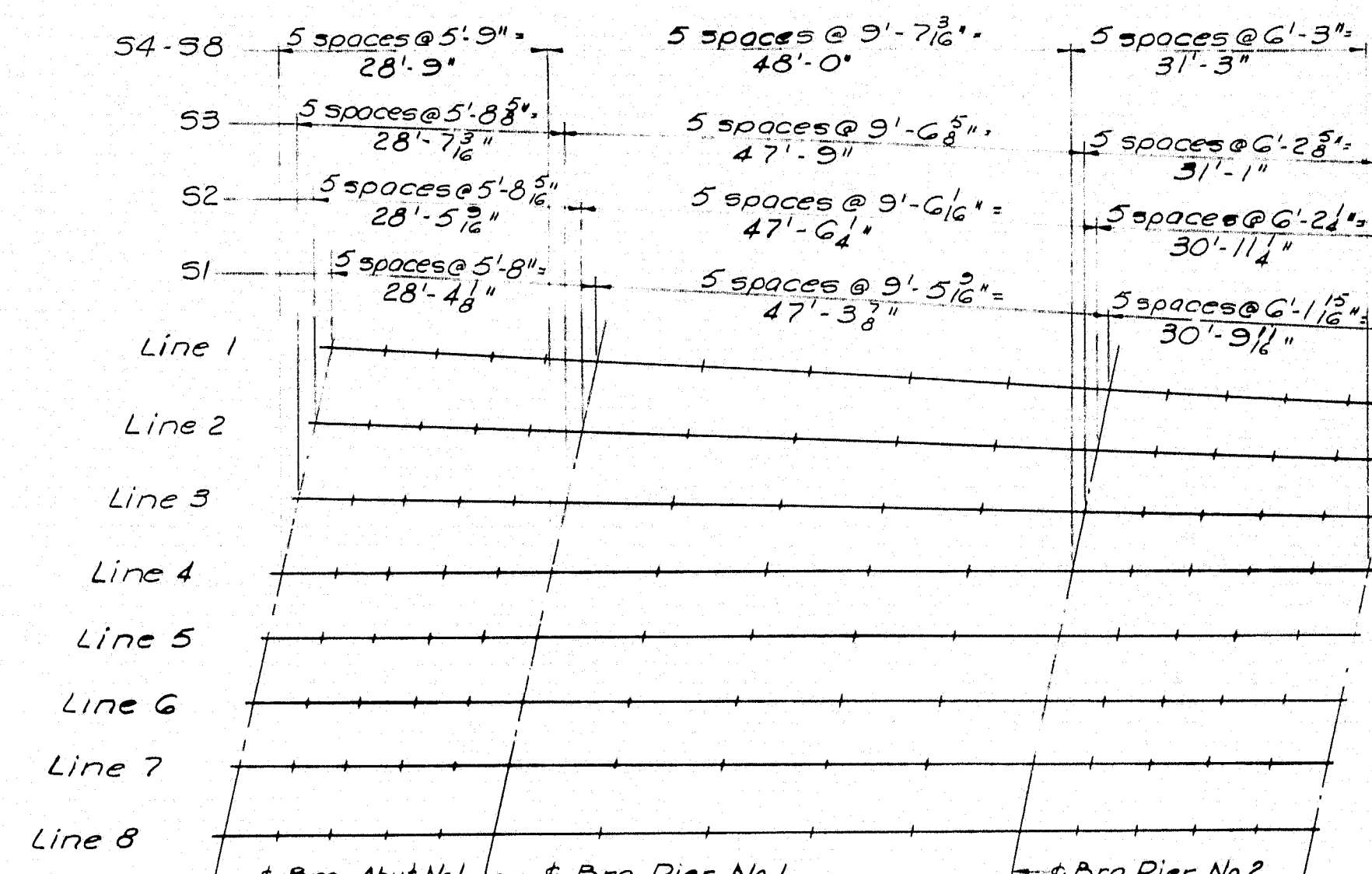
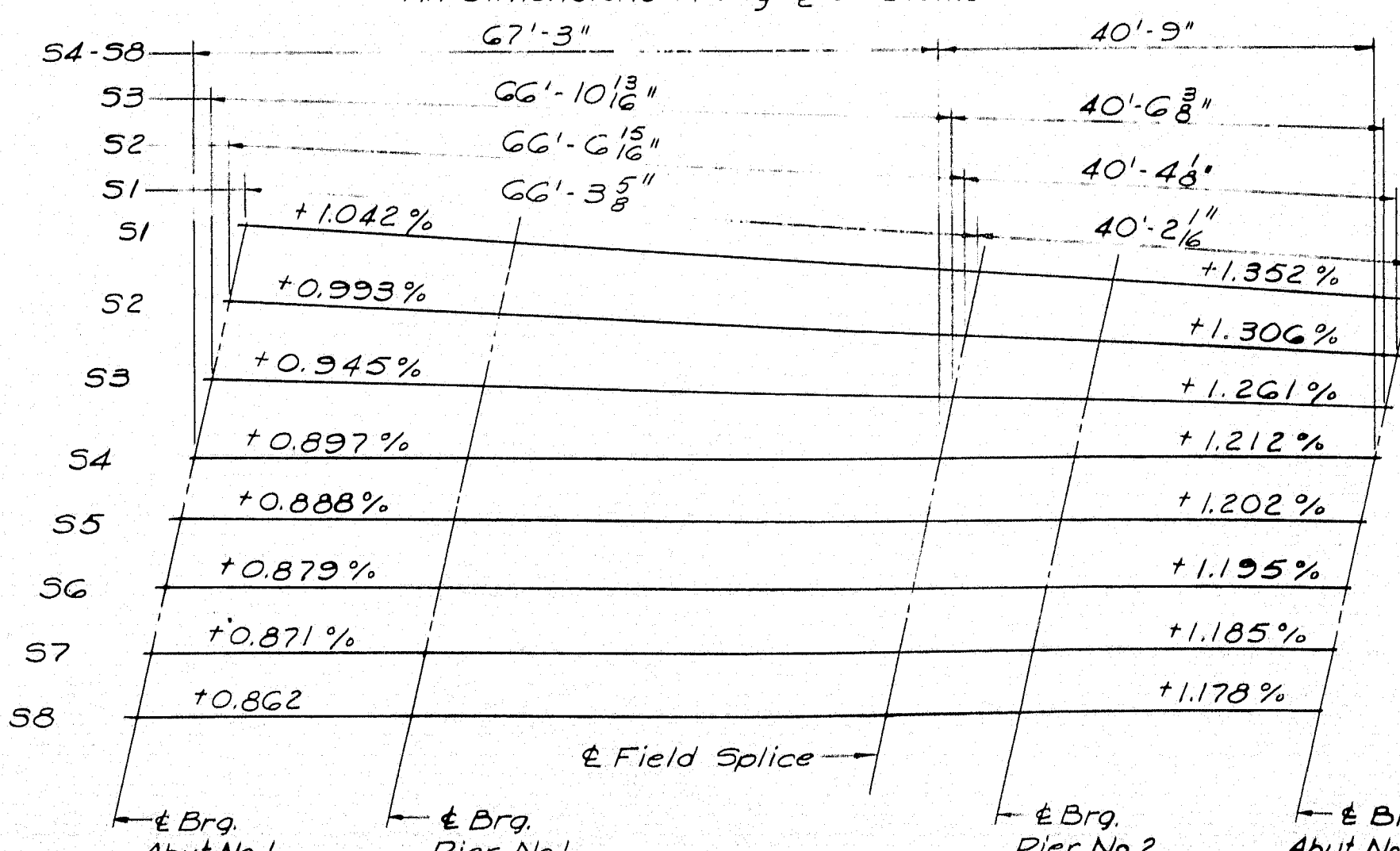


DIAGRAM OF BLOCKING POINTS

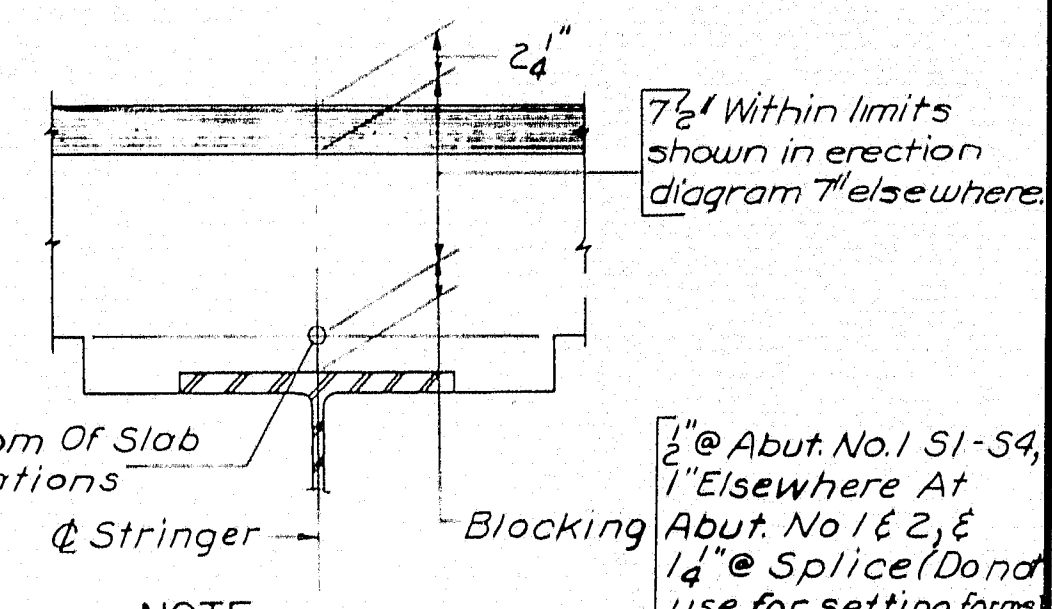
All Dimensions Along & Of Beams



BEAM GRADES

REFERENCE
Splice - See Standard Details BD 103-64
Diaphragms - See Standard Details BD 104-64
Pedestals - See Standard Details BD 101-64
Armored Joint - See Standard Details BD 104-64
Bridge Drain - See Standard Details BD 104-64

SPECIFICATIONS
Fabrication and Erection: State of Maine
Standard Specifications, Highway and
Bridges, Revision of Jan. 1956 and
Supplemental Specifications of Feb. 1960
Design and Detail: A.A.S.H.O. Standard
Specifications for Highway Bridges of 1961 and
Interim Specifications of 1961, 1962, 1963 & 1964.
Materials: Except as otherwise noted
on the Standard Details, all materials
shall conform to A.S.T.M. Designation A-36



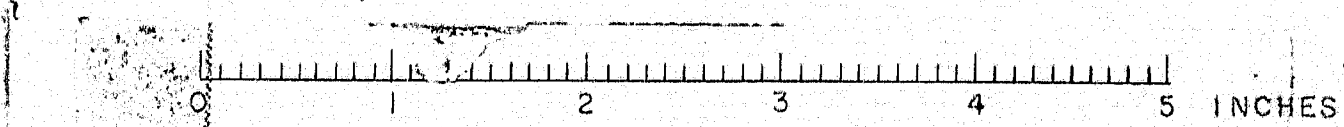
NOTE:
To compensate for dead load deflections
as well as possible irregularities in beams,
set the bottom of slab elevation at the
points indicated before any of the slab
formwork is started.

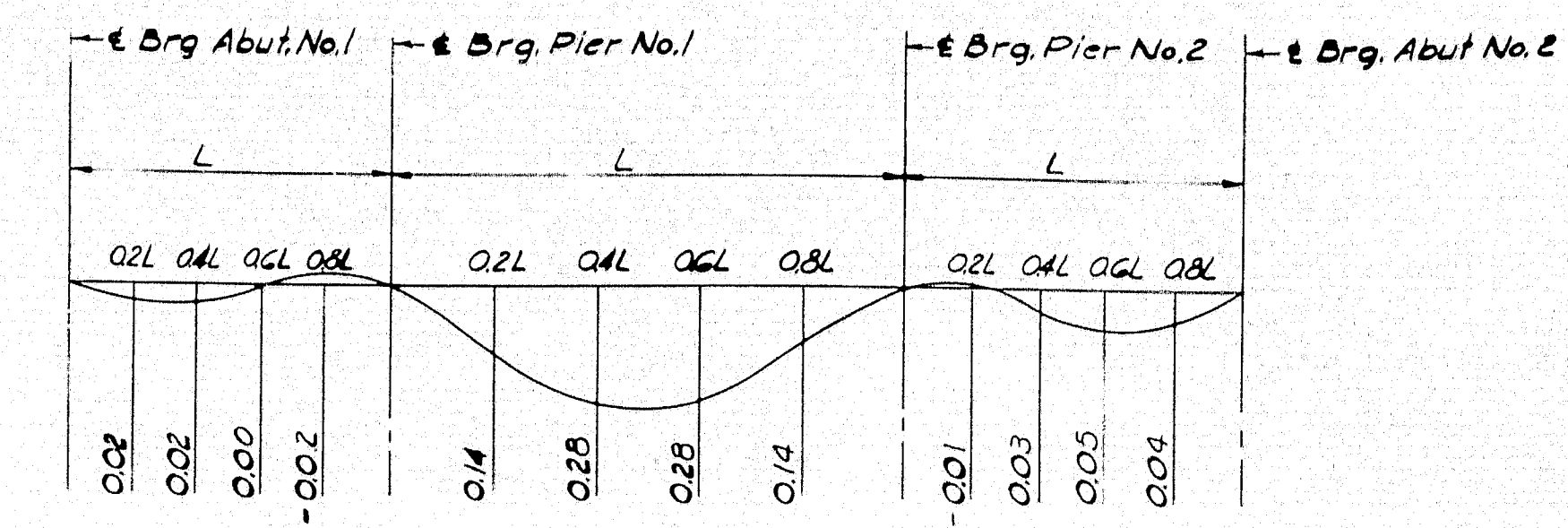
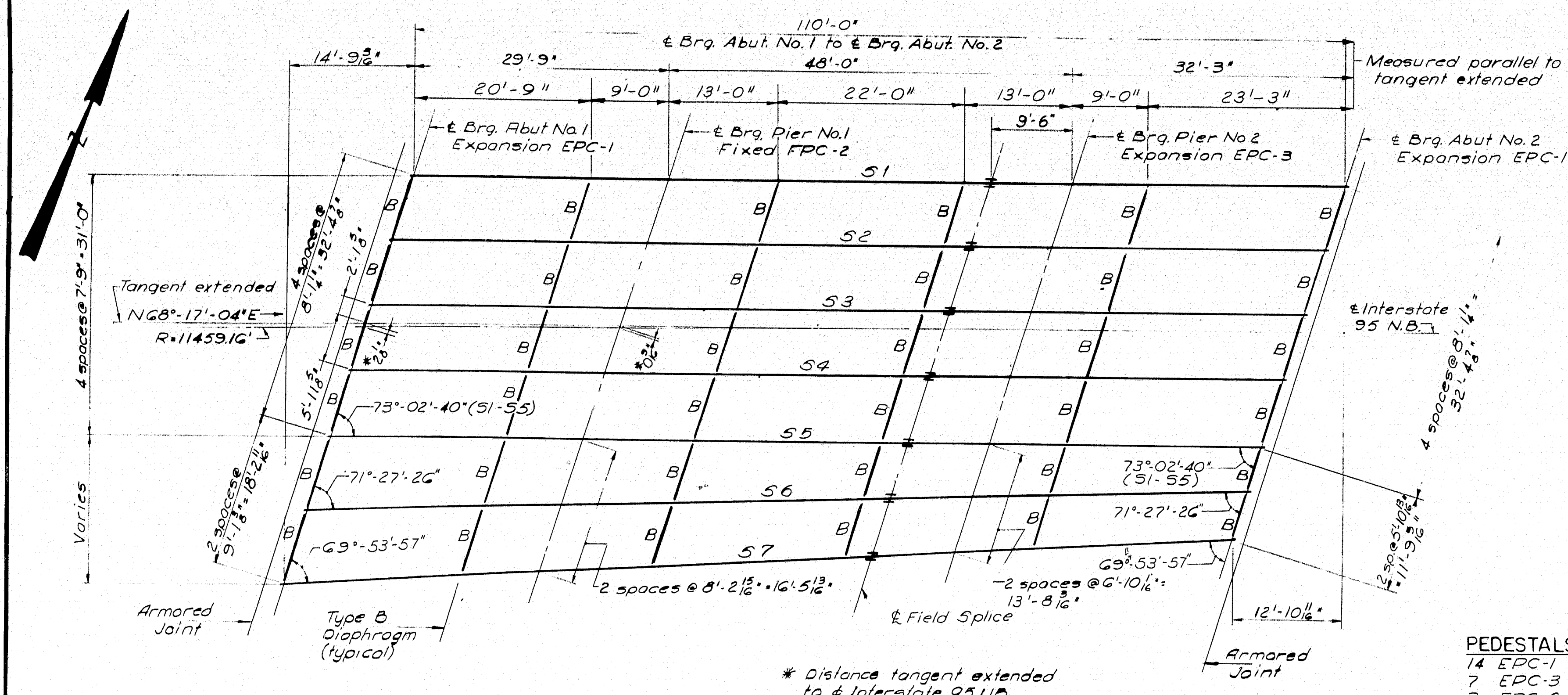
BLOCKING DETAIL

No Scale

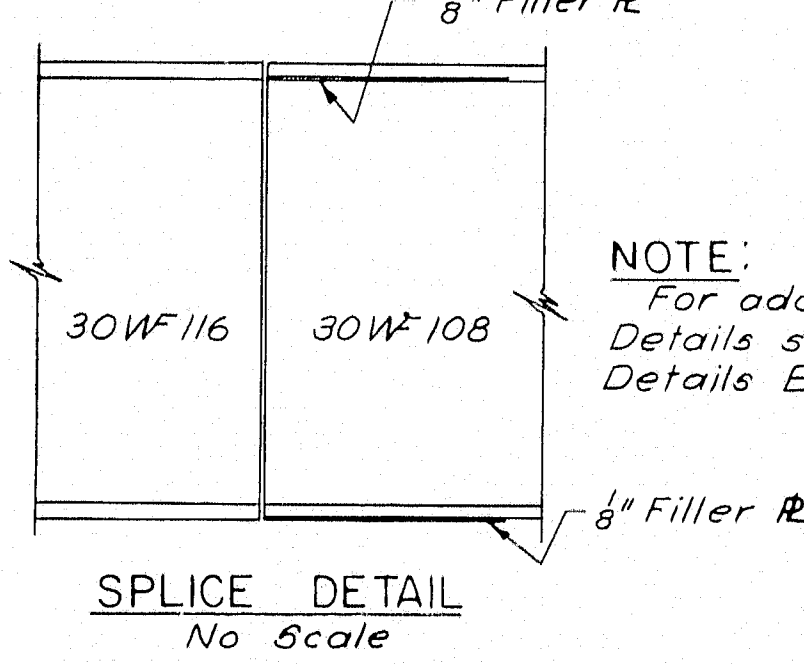
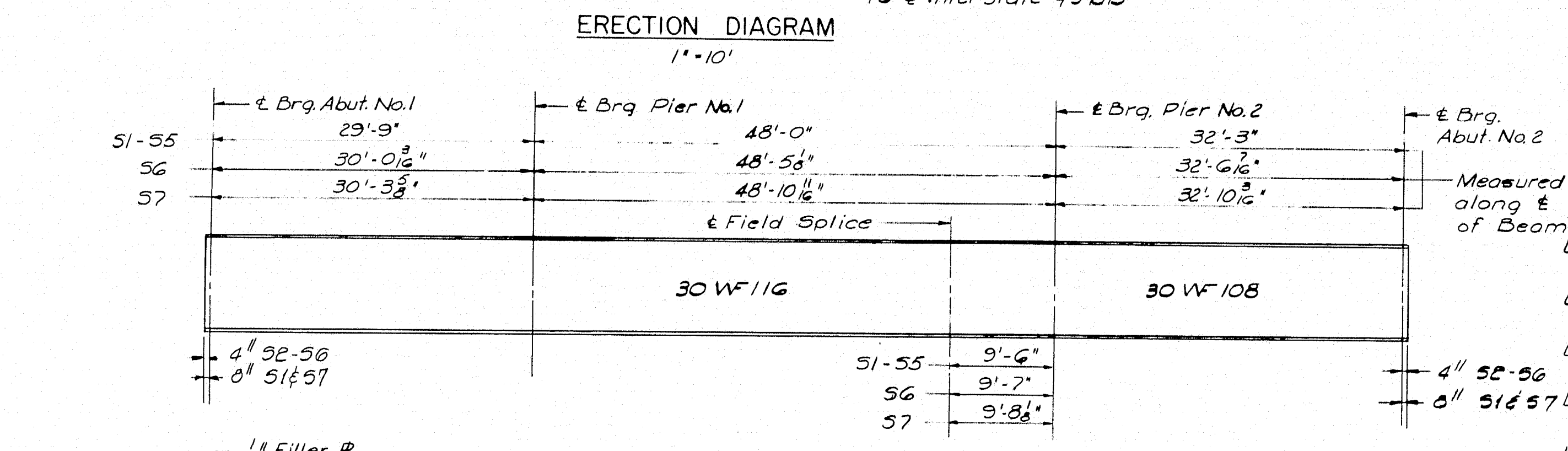
DESIGN - G.H. DETAIL - A.A.L.
TRACE - S.M.
CHECK - S.M.
BRIDGE NO. SURVEY - 109
STATE HIGHWAY COMMISSION
BRIDGE DIVISION
INTERSTATE 95 SB
OVER
RELOCATED EAST BRANCH
MATTAWAMKEAG RIVER
IN THE TOWN OF
OAKFIELD
ARROOSTOOK COUNTY
STRUCTURAL STEEL & BLOCKING
SHEET 10 OF 16 AUGUSTA, MAINE FEBRUARY 1965
DYER BROOK OAKFIELD (12)

M-2264

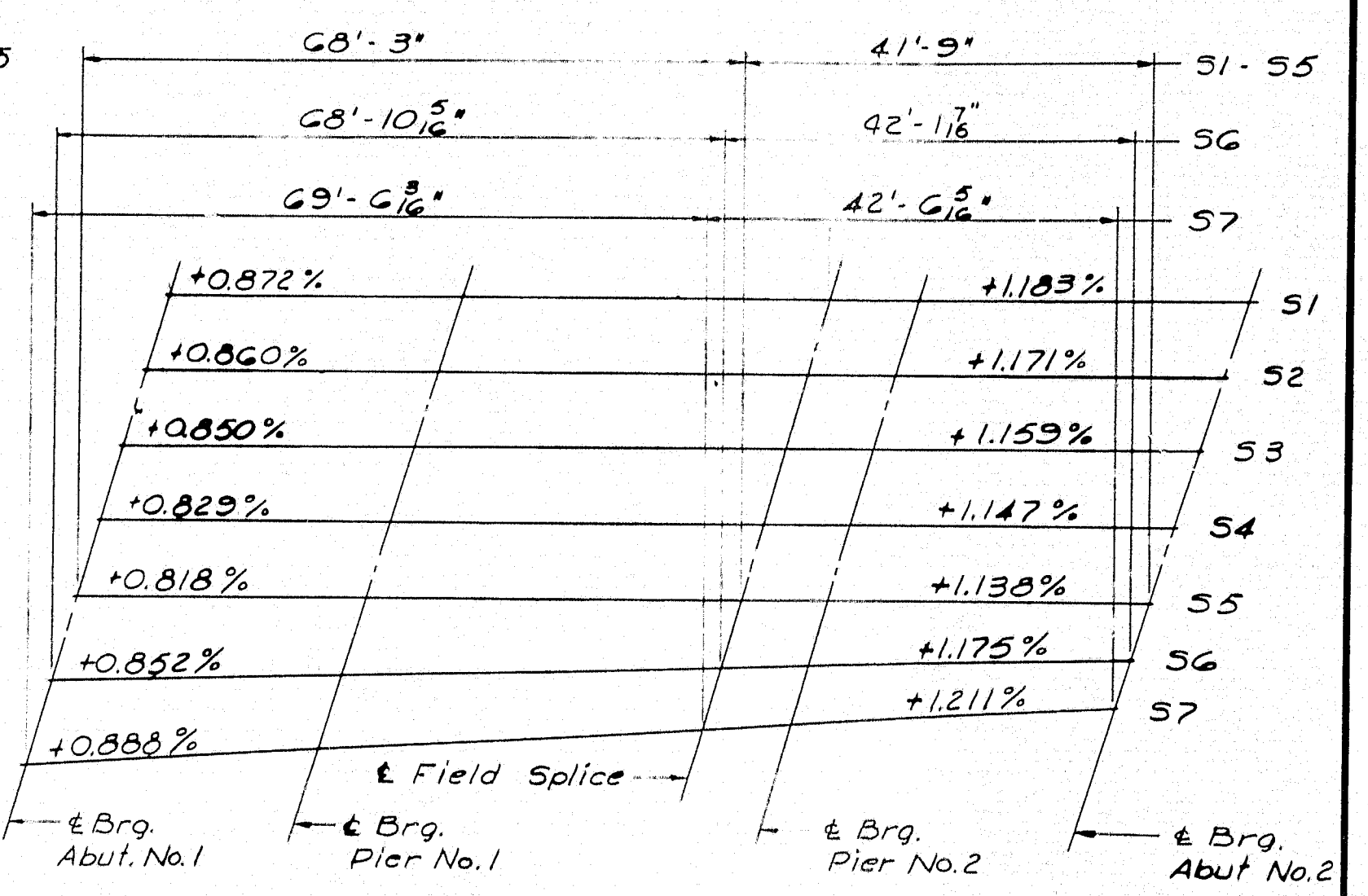
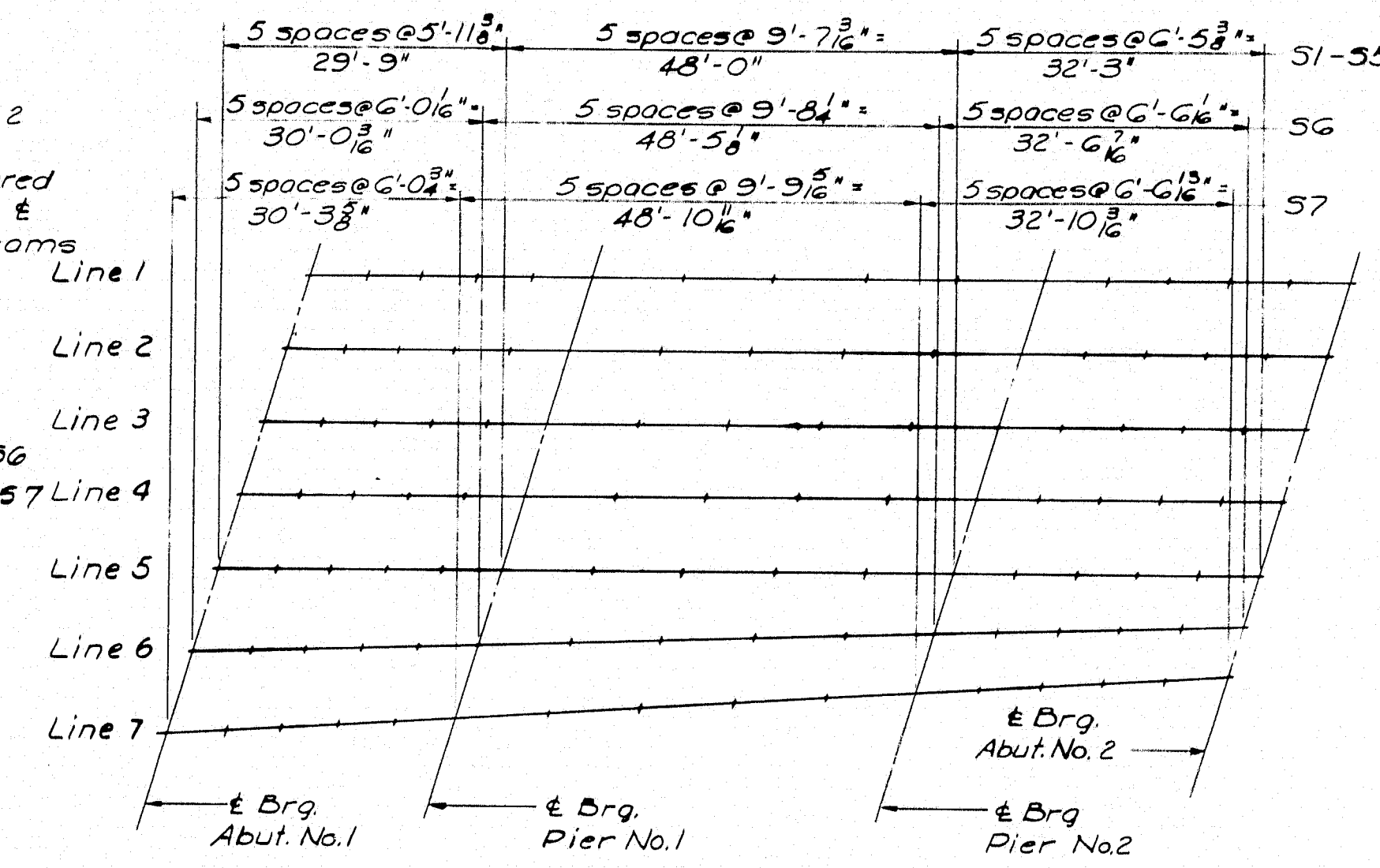




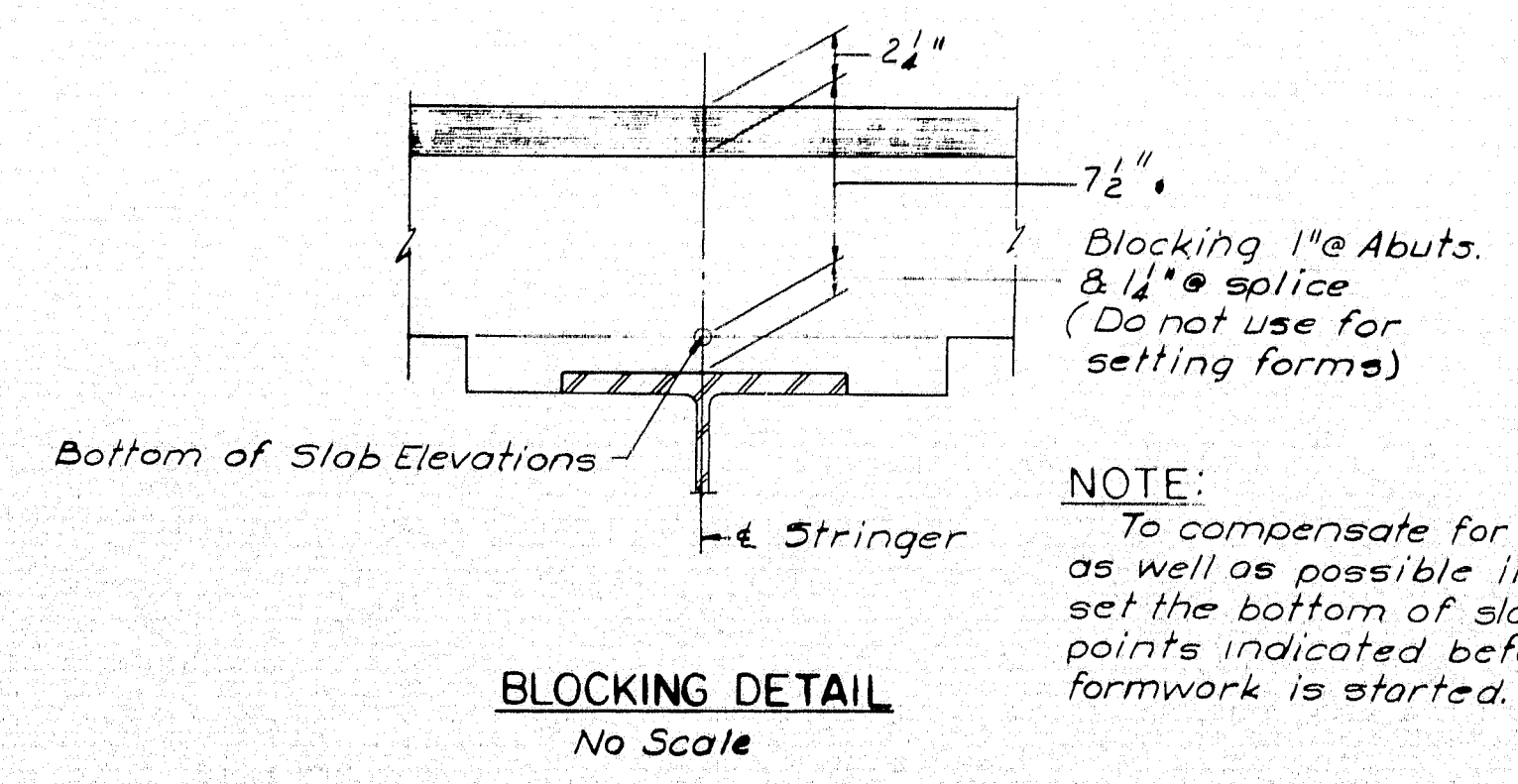
REFERENCE
 Splice - See Standard Details BD-103-G4
 Diaphragms - See Standard Details BD-104-G4
 Pedestals - See Standard Details BD-101-G4
 Armored Joint - See Standard Details BD-104-G4
 Bridge Drains - See Standard Details BD-104-G4
SPECIFICATIONS
 Fabrication and Erection: State of Maine Standard Specifications, Highway and Bridges, Revision of Jan. 1956 and Supplemental Specifications of Feb. 1960.
 Design and Detail: A.A.S.H.O. Standard Specifications for Highway Bridges of 1961 and Interim Specifications of 1961, 1962, 1963 & 1964.
 Materials: Except as otherwise noted on the Standard Details, all materials shall conform to A.S.T.M. Designation A-36



PEDESTALS
 14 EPC-1 Required
 7 EPC-3 Required
 7 FPC-2 Required



	Abut. No. 1	SPAN NO. 1					Abut. No. 2	SPAN NO. 2					Abut. No. 3	SPAN NO. 3					Abut. No. 4
	51-55	51-55	51-55	51-55	51-55	51-55	51-55	51-55	51-55	51-55	51-55	51-55	51-55	51-55	51-55	51-55	51-55	51-55	51-55
Line 1	561.20	561.24	561.29	561.33	561.38	561.43	561.53	561.63	561.73	561.81	561.91	561.98	562.05	562.13	562.21	562.29	562.39	562.49	562.59
Line 2	561.32	561.36	561.41	561.45	561.50	561.55	561.64	561.74	561.84	561.92	562.01	562.09	562.16	562.24	562.32	562.39	562.49	562.59	562.69
Line 3	561.43	561.48	561.52	561.57	561.61	561.66	561.76	561.86	561.95	562.03	562.12	562.19	562.27	562.35	562.42	562.50	562.59	562.69	562.79
Line 4	561.36	561.40	561.44	561.49	561.53	561.58	561.67	561.77	561.86	561.95	562.03	562.10	562.18	562.25	562.33	562.40	562.49	562.59	562.69
Line 5	561.21	561.25	561.29	561.33	561.38	561.43	561.52	561.62	561.70	561.79	561.87	561.94	562.02	562.09	562.17	562.24	562.33	562.43	562.53
Line 6	561.04	561.08	561.13	561.17	561.22	561.27	561.37	561.47	561.56	561.65	561.74	561.81	561.89	561.97	562.04	562.12	562.21	562.31	562.41
Line 7	560.87	560.92	560.96	561.01	561.06	561.11	561.21	561.32	561.42	561.51	561.61	561.68	561.76	561.84	561.92	562.00	562.09	562.19	562.29
56	6'-0.16"	12'-0.16"	18'-0.16"	24'-0.16"	30'-0.16"	36'-0.16"	42'-0.16"	48'-0.16"	54'-0.16"	60'-0.16"	66'-0.16"	72'-0.16"	78'-0.16"	84'-0.16"	90'-0.16"	96'-0.16"	102'-0.16"	108'-0.16"	114'-0.16"
57	6'-0.16"	12'-0.16"	18'-0.16"	24'-0.16"	30'-0.16"	36'-0.16"	42'-0.16"	48'-0.16"	54'-0.16"	60'-0.16"	66'-0.16"	72'-0.16"	78'-0.16"	84'-0.16"	90'-0.16"	96'-0.16"	102'-0.16"	108'-0.16"	114'-0.16"



DESIGN - G.H. DETAIL A.A.L.
 TRACE - S.M.
 CHECK - S.M.

BRIDGE NO. SURVEY - PLOT -

STATE HIGHWAY COMMISSION
 BRIDGE DIVISION

INTERSTATE 95 NB.
 OVER
 RELOCATED EAST BRANCH
 MATTAWAKEAG RIVER

IN THE TOWN OF
 OAKFIELD
 AROOSTOOK COUNTY

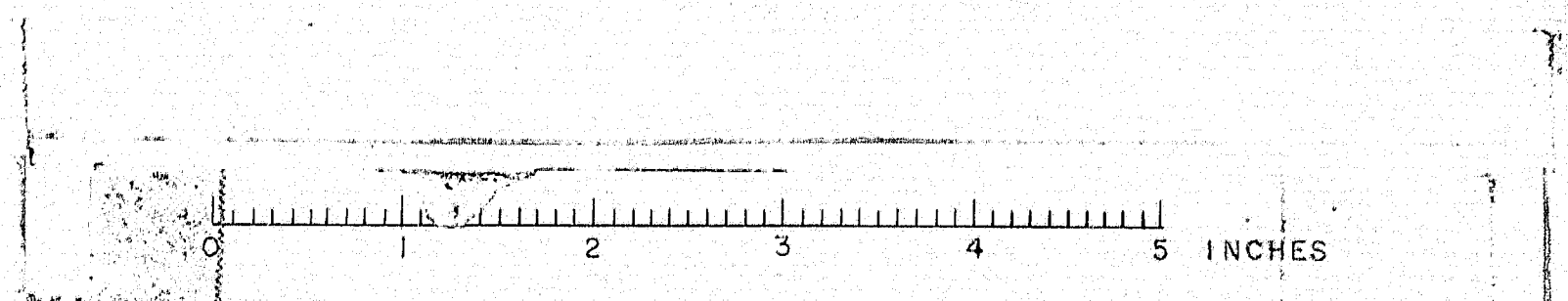
STRUCTURAL STEEL & BLOCKING

HOWARD, NEEDLES, TAMMEN & BERGENDOFF
 CONSULTING ENGINEERS

NEW YORK BOSTON KANSAS CITY

SHEET 11 OF 16 AUGUSTA, MAINE FEBRUARY 1965

M-2265 DYER BROOK OAKFIELD (12)



NOTE: Because of the Monolithic Placement of the Concrete Slab, the Construction Joints were Eliminated and the Reinforcing Either Spliced or Placed Continuously. The Concrete Haunch Over the B Diaphragm was Also Eliminated.

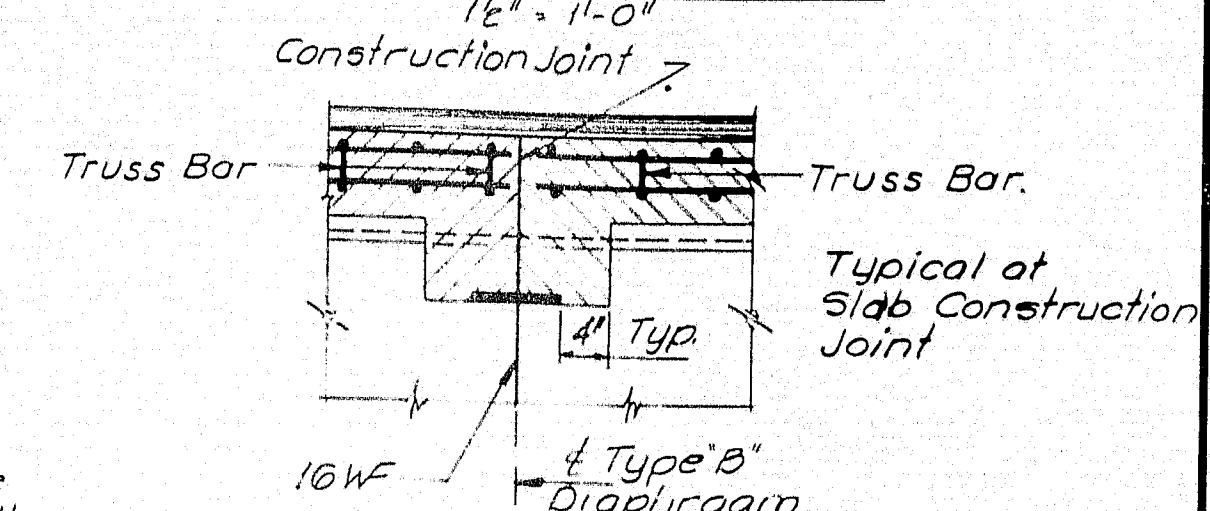
NOTE: Each panel starts with S505 & ends with S502 except at A₃.

TYPICAL REINFORCING PATTERN

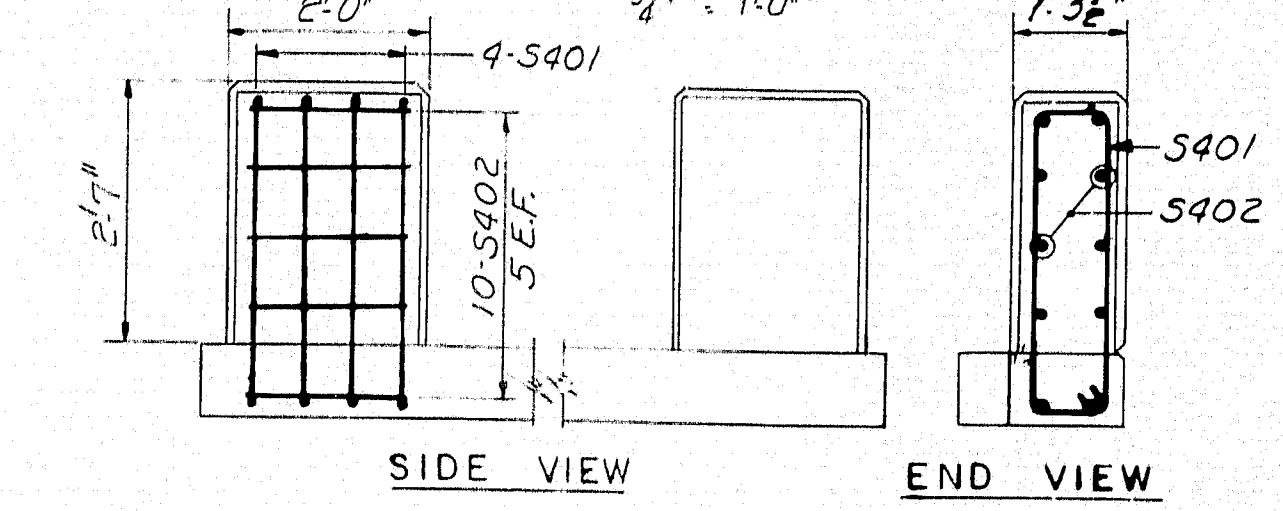
Sequence of Anchor Rod indicates Grouted into Granite.

Membrane Waterproofing (Shown with Bituminous Concrete & 1" Allowed For Membrane Waterproofing (By Others))

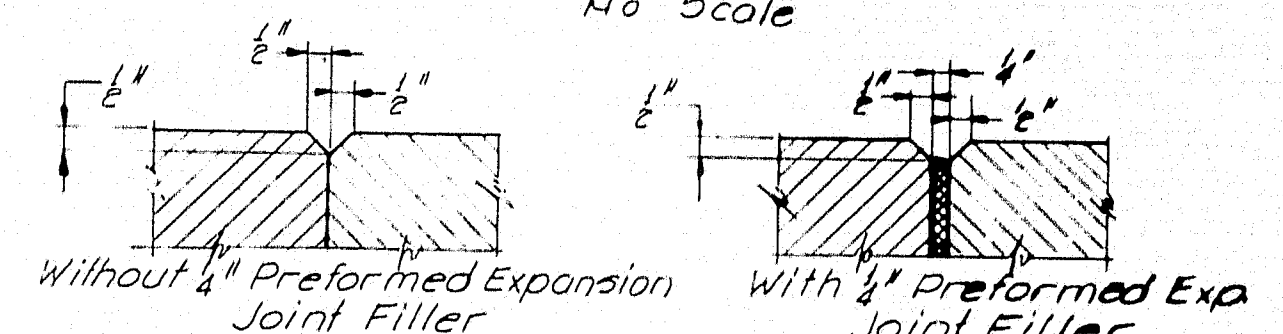
TYPICAL CURB SECTION



SECTION B-B



END POST DETAIL



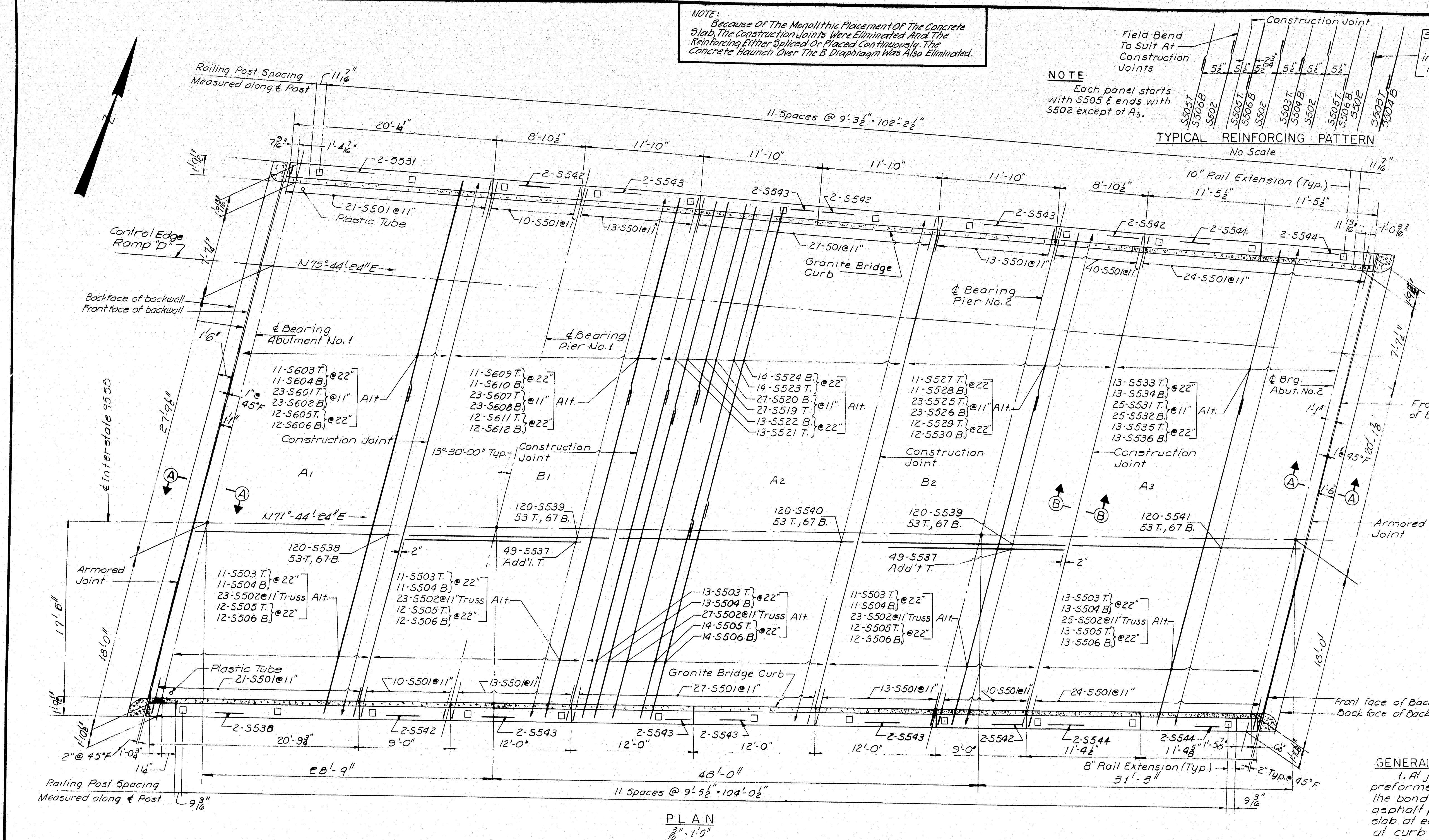
1" V-GROOVES

GENERAL SUPERSTRUCTURE NOTES:

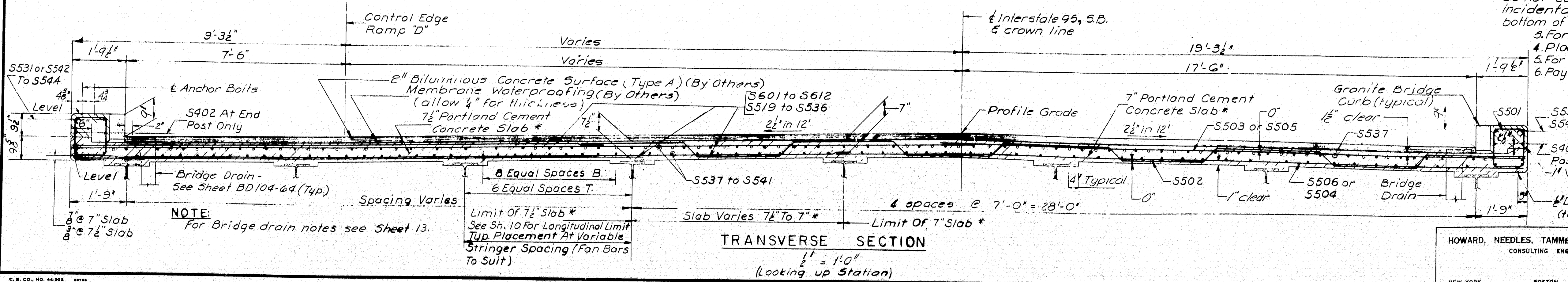
- At joints in curbs and granite bridge curbs over piers, use 1" preformed expansion joint filler. At all other curb joints, break the bond between concrete surfaces with a suitable grade of asphalt paint. Form 1" V-groove on outside face of curb and slab at each vertical joint. Provide joints in granite bridge curb at curb construction joints.
- At low points in slabs, place a plastic tube 1" through the slab for drainage. Exact location to be determined in the field. Do not cover the tube with waterproofing. This work will be incidental to contract items. Tubes shall extend 2" below bottom of slab. Place tubes to drip clear of bridge seat.
- For bridge rail, see Standard Detail 13, 10107-64, 10103-64.
- Place concrete in A panels before placing concrete in B panels.
- For Section A-A & Armored Joint Layout, see Sheet 13.
- Payment for concrete end posts shall be made under Item 701-40.

DESIGN - G.H. DETAIL - D.A.T.
TRACE - V.A.V.
BRIDGE NO. 95 S.B.
STATE HIGHWAY COMMISSION
BRIDGE DIVISION
INTERSTATE 95 S.B.
OVER
RELOCATED EAST BRANCH
MATTAWAKEAG RIVER
IN THE TOWN OF
OAKFIELD
AROSTOOK COUNTY
SUPERSTRUCTURE
SHEET 12 OF 16 AUGUSTA, MAINE FEBRUARY 1965
DYER BROOK OAKFIELD (12)

HOWARD, NEEDLES, TAMMEN & BERGENDOFF
CONSULTING ENGINEERS
NEW YORK BOSTON KANSAS CITY



PLAN

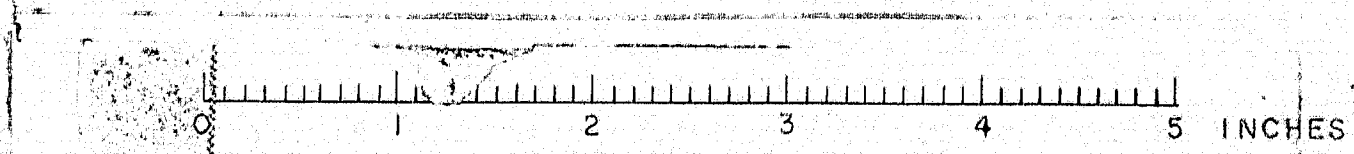


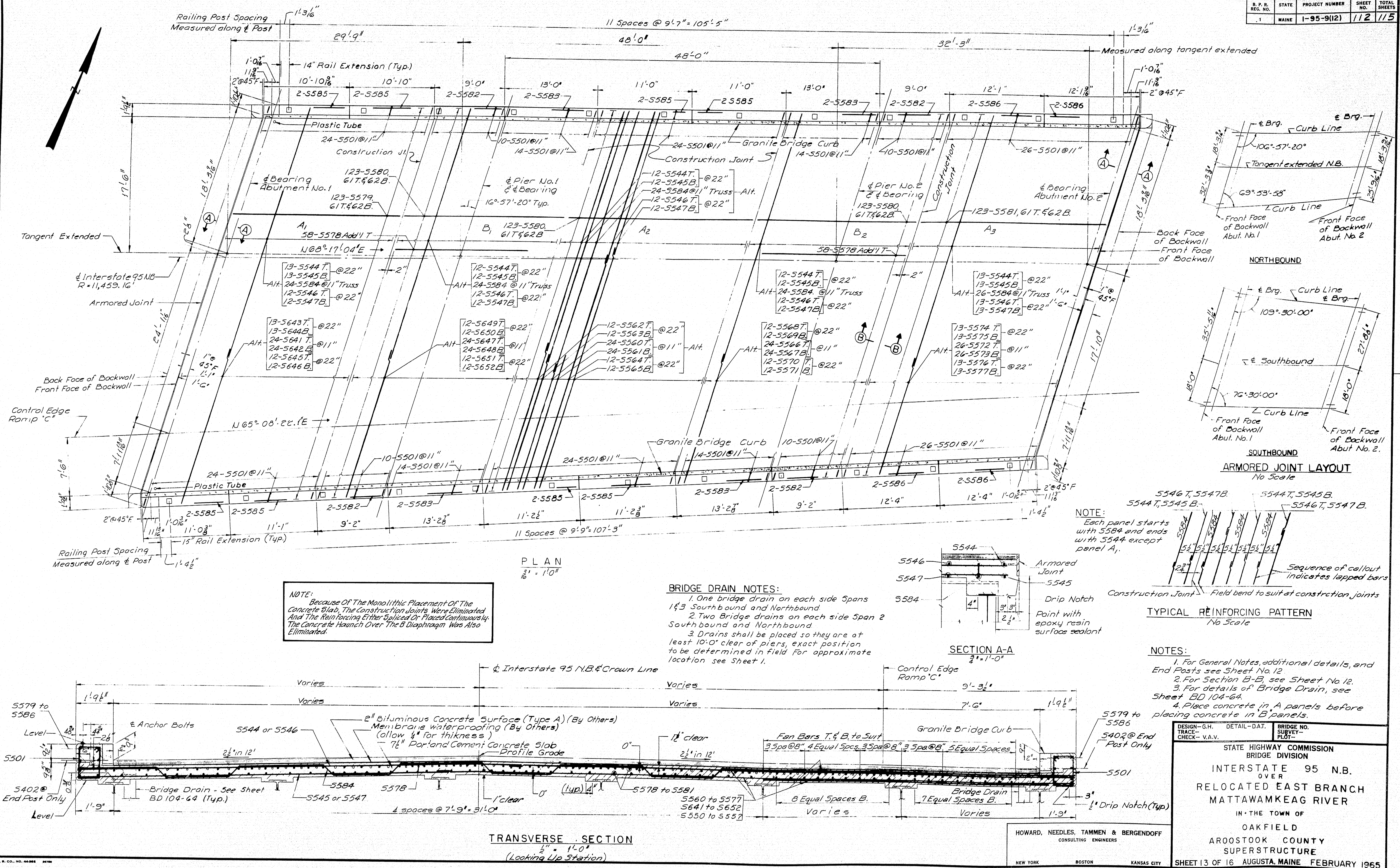
TRANSVERSE SECTION

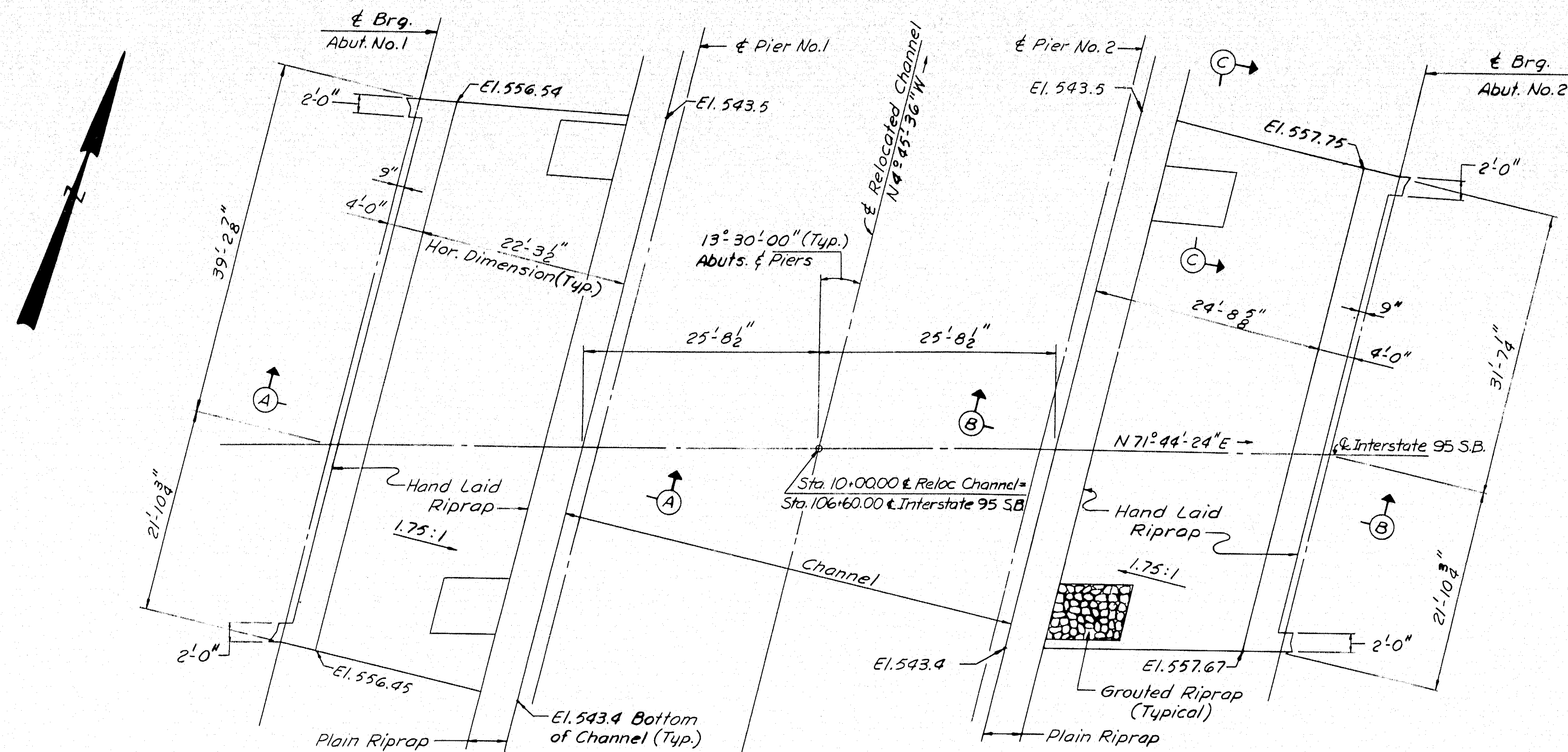
NOTE: For Bridge drain notes see Sheet 13.

Limit of 7 1/2" Slab *
See Sh. 10 For Longitudinal Limit
Typ. Placement At Variable
Stringer Spacing (Fan Bars
To Suit)

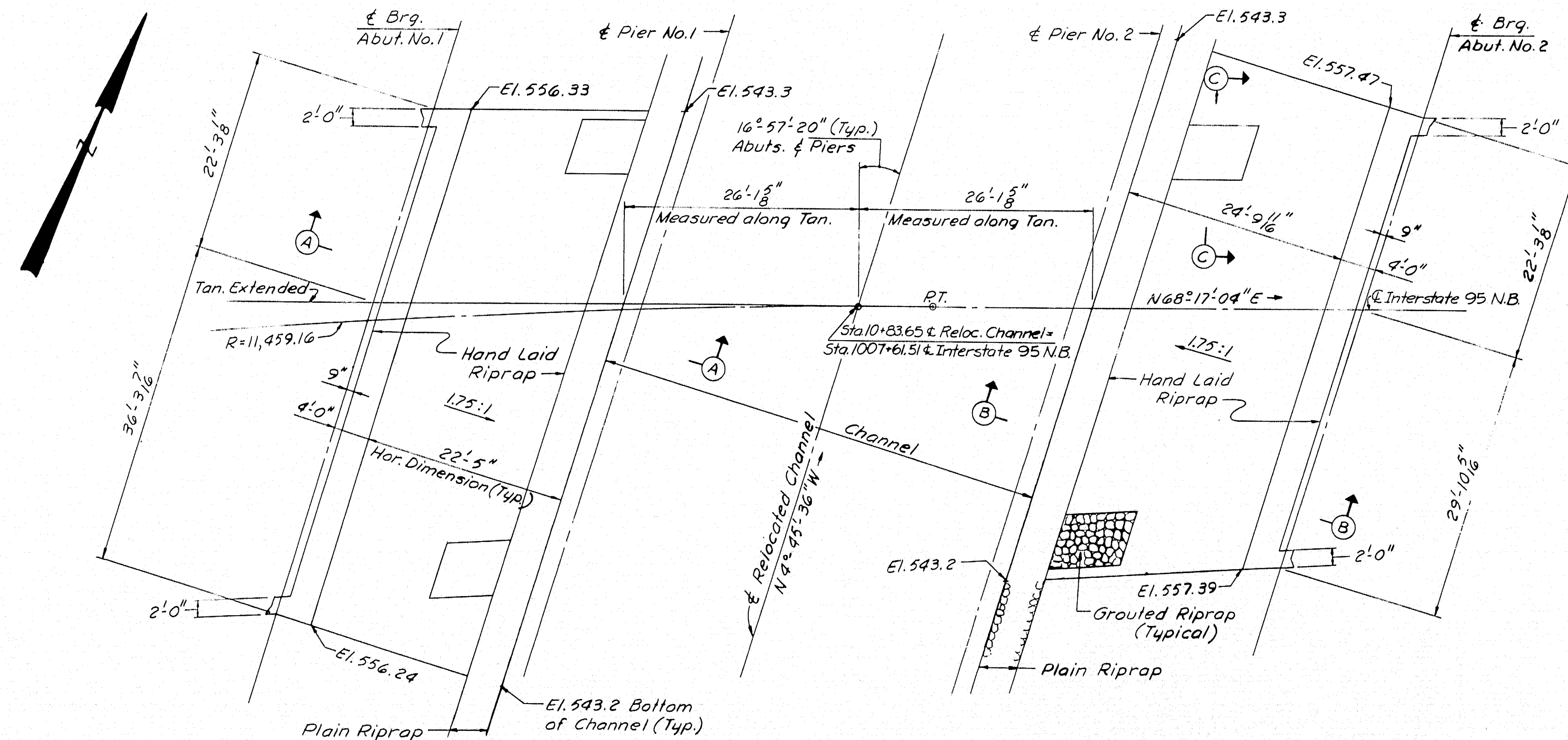
(Looking up Station)





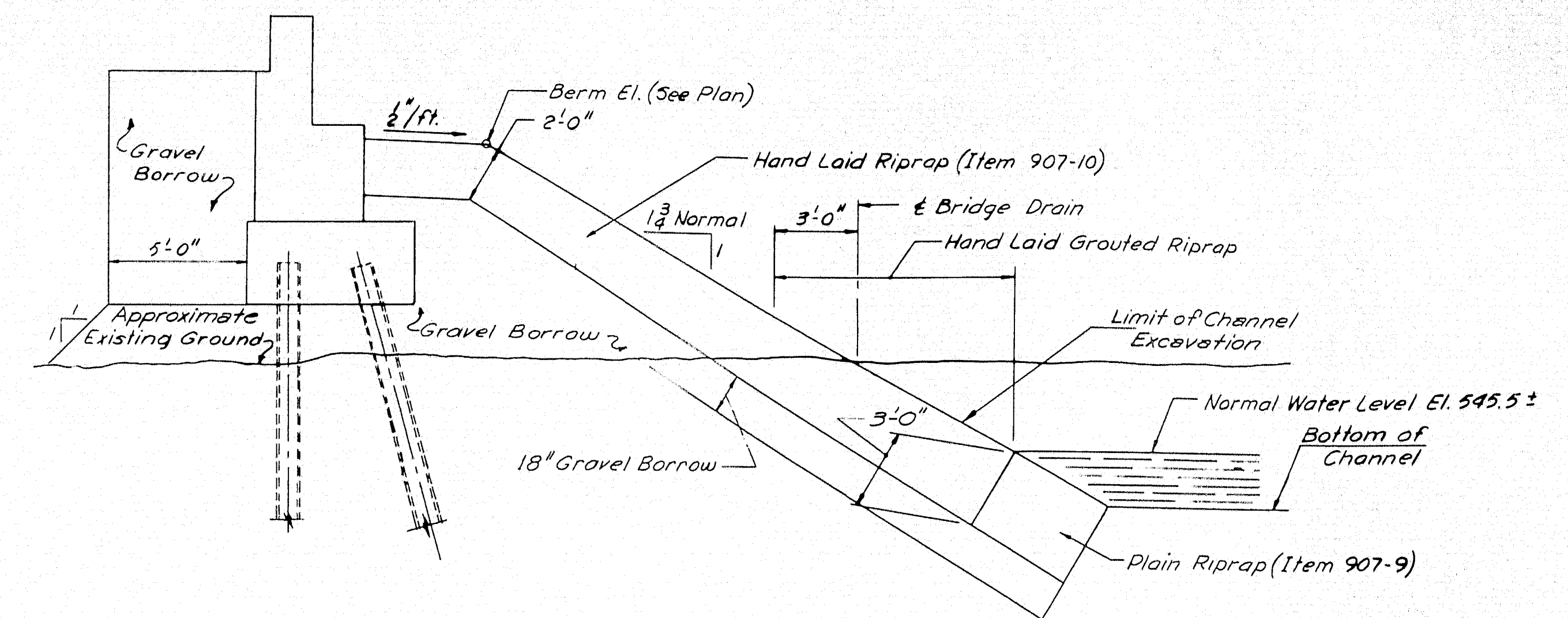


PLAN - SOUTHBOUND
1" = 10'



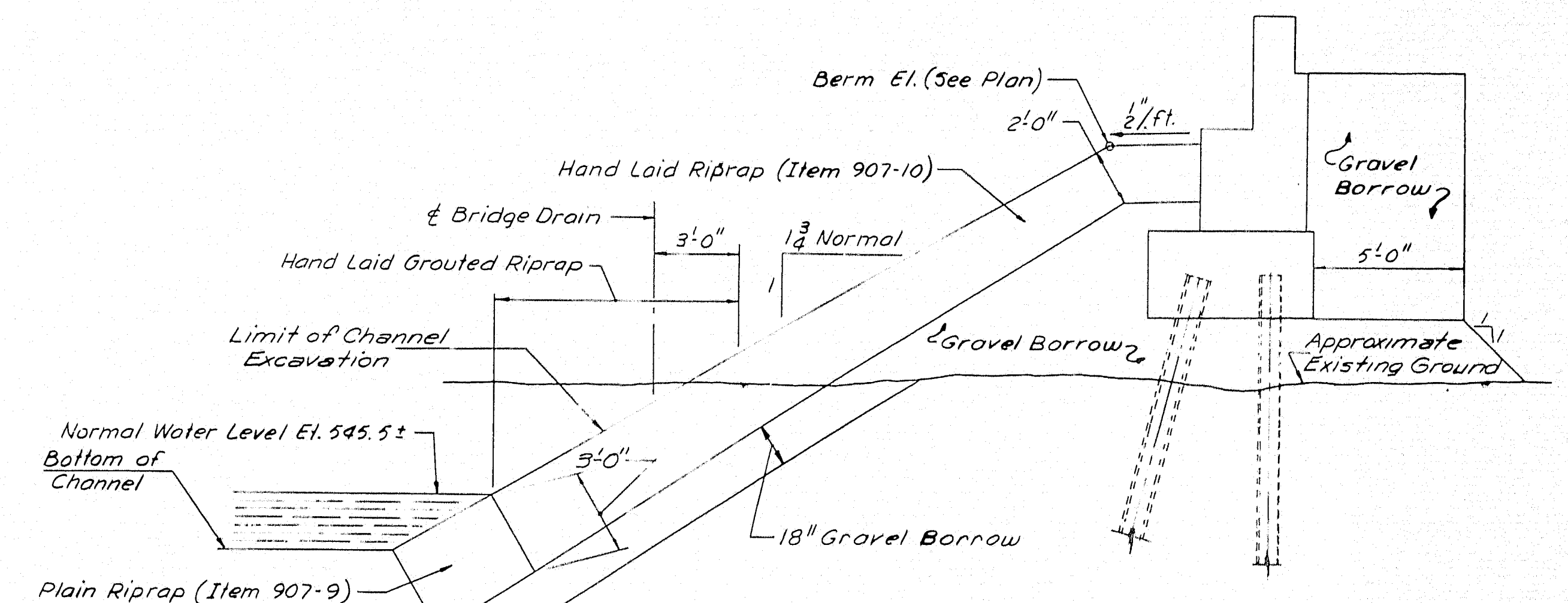
PLAN - NORTHBOUND
1" = 10'

NOTE:
For limits of Riprap see
General Plan, Sheet 1.



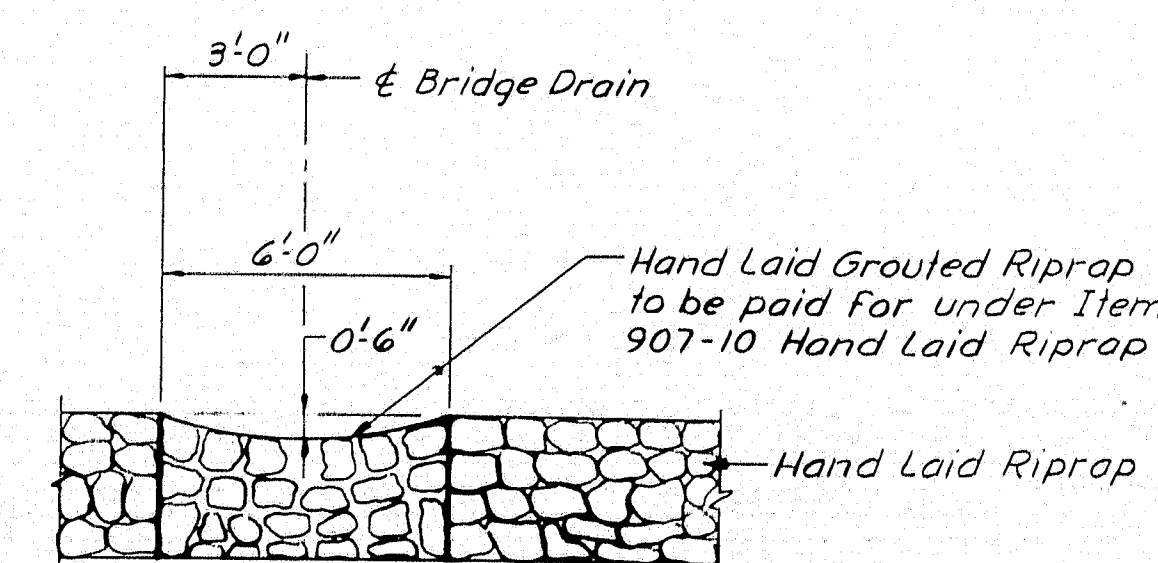
SECTION A-A
1/4" = 1'-0"

NOTE:
Payment for any excavation required
for slope protection will be made under
Item for Structural Earth Excavation
Piers Item 204-14.



SECTION B-B
1/4" = 1'-0"

NOTE:
The 18" of Gravel Borrow under
the Riprap may be reduced or omitted
if existing material is suitable.

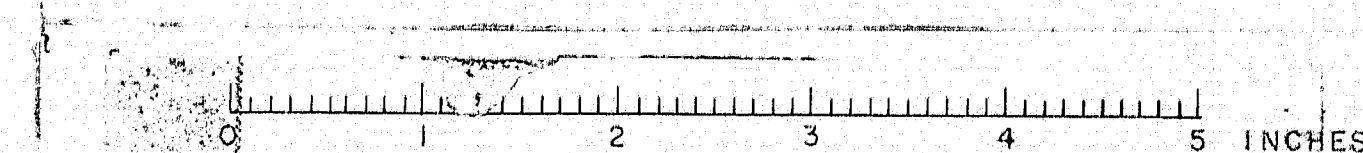


SECTION C-C
1/4" = 1'-0"

HOWARD, NEEDLES, TAMMEN & BERGENDOFF
CONSULTING ENGINEERS
NEW YORK BOSTON KANSAS CITY

DESIGN - TRACE - CHECK - S.M.	DETAIL - J.W.M.	BRIDGE NO. SURVEY - PLOT -
STATE HIGHWAY COMMISSION BRIDGE DIVISION INTERSTATE 95 OVER RELOCATED EAST BRANCH MATTAWAMKEAG RIVER IN THE TOWN OF OAKFIELD AROSTOOK COUNTY SLOPE PROTECTION		
SHEET 14 OF 16 AUGUSTA, MAINE FEBRUARY 1965		

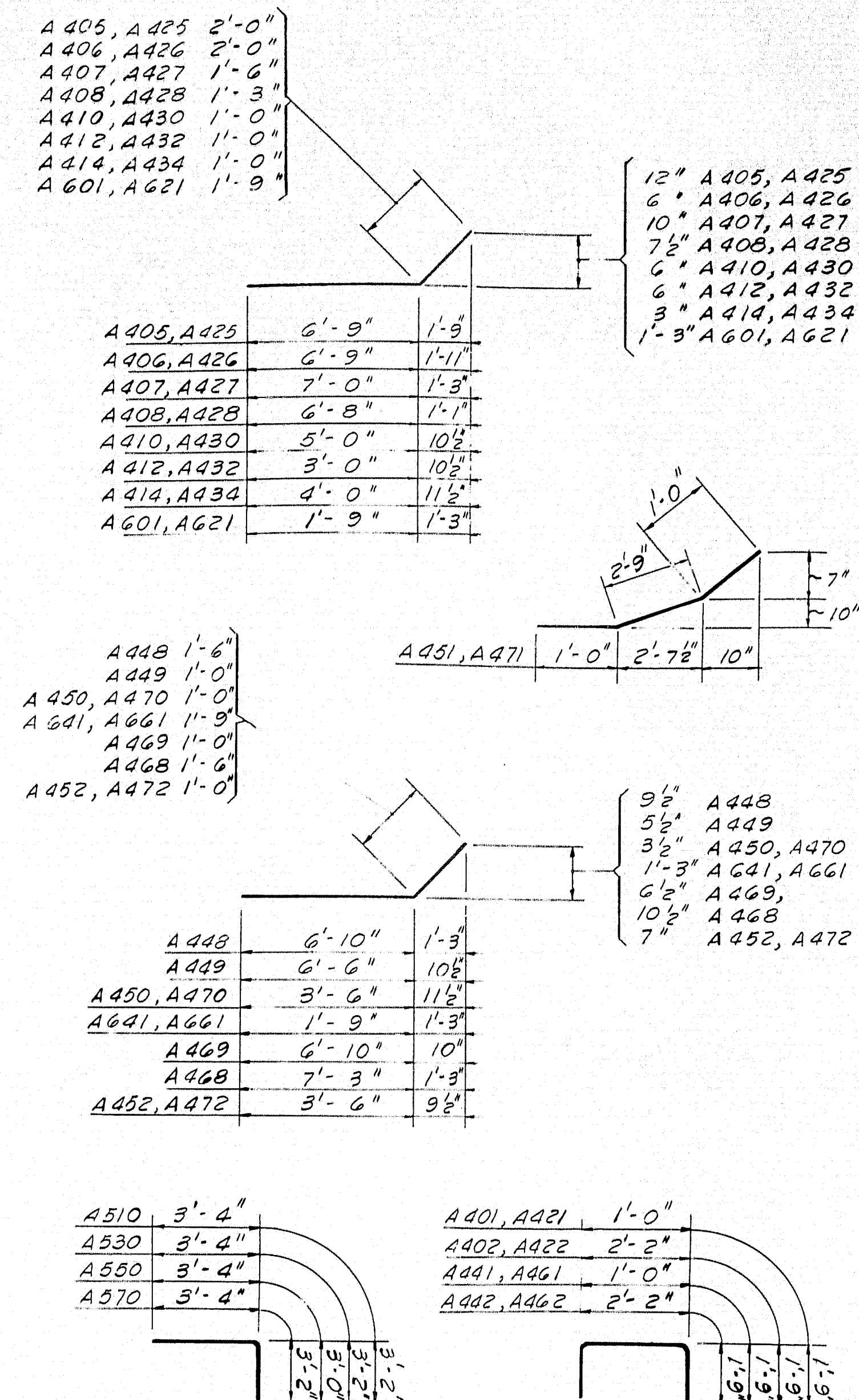
M-2268 DYER BROOK OAKFIELD (12)



ABUTMENT 1 (Southbound)				
MARK	SIZE	NUMBER	LENGTH	INCR.
STRAIGHT BARS				
A403	4	12	30'-0"	
A404	4	8	8'-6"	
A409	4	1	6'-0"	
A411	4	1	4'-0"	
A413	4	1	5'-0"	
A501	5	16	28'-9"	
A502	5	78	4'-0"	
A503	5	39	3'-0"	
A504	5	39	6'-0"	
A505	5	71	2'-6"	
A506	5	2	8'-1"	
A507	5	2	8'-4"	
A508	5	14	4'-4"	
A509	5	14	3'-2"	
A602	6	110	5'-6"	
A603	6	24	29'-0"	
A604	6	10	10'-0"	
A605	6	10	10'-6"	
A606	6	16	3'-6"	
A607	6	18	3'-6"	
A608	6	4	1'-0"	
BENT BARS				
A401	4	16	4'-6"	
A402	4	16	5'-8"	
A405	4	4	8'-9"	
A406	4	4	8'-9"	
A407	4	2	8'-6"	
A408	4	2	7'-11"	
A410	4	1	6'-0"	
A412	4	1	4'-0"	
A414	4	1	5'-0"	
A510	5	39	6'-6"	
A601	6	39	3'-6"	
ABUTMENT 2 (Southbound)				
STRAIGHT BARS				
A423	4	12	26'-3"	
A424	4	8	8'-6"	
A429	4	1	6'-0"	
A431	4	1	4'-0"	
A433	4	1	5'-0"	
A521	5	16	25'-0"	
A522	5	68	4'-0"	
A523	5	34	3'-0"	
A524	5	34	6'-0"	
A525	5	66	2'-6"	
A526	5	2	7'-10"	
A527	5	2	7'-9"	
A528	5	14	4'-4"	
A529	5	14	7'-10"	
A622	6	96	5'-6"	
A623	6	24	25'-3"	
A624	6	10	10'-0"	
A625	6	10	10'-6"	
A626	6	16	3'-6"	
A627	6	18	3'-6"	
A628	6	4	1'-0"	

ABUTMENT 2 (Southbound) Cont.				
MARK	SIZE	NUMBER	LENGTH	INCR.
BENT BARS				
A421	4	16	4'-6"	
A422	4	16	3'-8"	
A425	4	4	8'-9"	
A426	4	4	8'-9"	
A427	4	2	8'-6"	
A428	4	2	7'-11"	
A430	4	1	6'-0"	
A432	4	1	4'-0"	
A434	4	1	5'-0"	
A530	5	34	6'-6"	
A621	6	34	3'-6"	
ABUTMENT 1 (Northbound)				
STRAIGHT BARS				
A443	4	12	26'-0"	
A444	4	16	6'-9"	
A445	4	2	5'-0"	
A446	4	2	3'-0"	
A447	4	2	4'-0"	
A541	5	16	27'-6"	
A542	5	74	4'-0"	
A543	5	37	3'-0"	
A544	5	37	6'-0"	
A545	5	69	2'-6"	
A546	5	2	8'-3"	
A547	5	2	7'-11"	
A548	5	14	4'-6"	
A549	5	14	4'-9"	
A642	6	104	5'-6"	
A643	6	24	27'-9"	
A644	6	10	11'-0"	
A645	6	10	10'-0"	
A646	6	18	3'-6"	
A647	6	16	6'-0"	
A648	6	4	1'-0"	
BENT BARS				
A441	4	14	4'-6"	
A442	4	14	5'-8"	
A448	4	2	8'-4"	
A449	4	2	7'-6"	
A450	4	12	4'-6"	
A451	4	6	4'-9"	
A452	4	6	4'-6"	
A550	5	37	6'-4"	
A641	6	36	3'-6"	
ABUTMENT 2 (Northbound)				
STRAIGHT BARS				
A463	4	12	22'-10"	
A464	4	16	6'-9"	
A465	4	2	5'-0"	
A466	4	2	3'-0"	
A467	4	2	4'-0"	
A561	5	16	24'-4"	
A562	5	66	4'-0"	
A563	5	33	3'-0"	
A564	5	33	6'-2"	
A565	5	65	2'-6"	
A566	5	2	8'-1"	
A567	5	2	8'-4"	

ABUTMENT 2 (Northbound) Cont.				
MARK	SIZE	NUMBER	LENGTH	INCR.
STRAIGHT BARS				
A568	5	14	4'-4"	
A569	5	14	5'-3"	
A662	6	92	5'-6"	
A663	6	24	24'-7"	
A664	6	10	11'-0"	
A665	6	10	10'-0"	
A666	6	18	3'-6"	
A667	6	16	3'-6"	
A668	6	4	1'-0"	
BENT BARS				
A461	4	14	4'-6"	
A462	4	14	5'-8"	
A468	4	2	8'-9"	
A469	4	2	7'-10"	
A470	4	12	4'-6"	
A471	4	6	4'-9"	
A472	4	6	4'-6"	
A570	5	33	6'-6"	
A661	6	32	3'-6"	
PIER 1 (Southbound)				
STRAIGHT BARS				
P503	5	32	28'-9"	
P504	5	32	25'-6"	
P601	6	8	30'-0"	
P602	6	8	25'-5"	
P604	6	120	17'-8"	
P605	6	120	4'-9"	
P606	6	10	30'-0"	
P607	6	10	31'-3"	
P701	7	61	8'-6"	
BENT BARS				
P401	4	28	9'-2"	
P501	5	32	2'-8"	
P502	5	64	4'-4"	

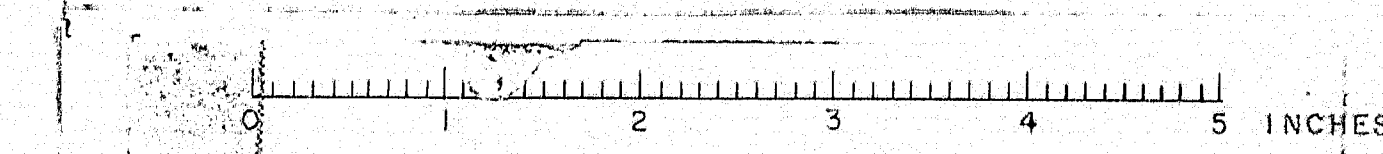


NOTES:
 1. All dimensions are to center of bars.
 2. All reinforcing bars shall be intermediate grade steel.
 3. Reinforcing steel to have 2" minimum cover unless otherwise shown.

DESIGN - G.H. DETAIL - J.R.A. BRIDGE NO. 114
 TRACE - CHECK - P.R.N. SURVEY - PLOT
 STATE HIGHWAY COMMISSION
 BRIDGE DIVISION
 INTERSTATE 95
 OVER
 RELOCATED EAST BRANCH
 MATTAWAMKEAG RIVER
 IN THE TOWN OF
 OAKFIELD
 AROOSTOOK COUNTY
 REINFORCING STEEL
 SHEET 15 OF 16 AUGUSTA, MAINE FEBRUARY 1965

HOWARD, NEEDLES, TAMMEN & BERGENDOFF
 CONSULTING ENGINEERS
 NEW YORK BOSTON KANSAS CITY

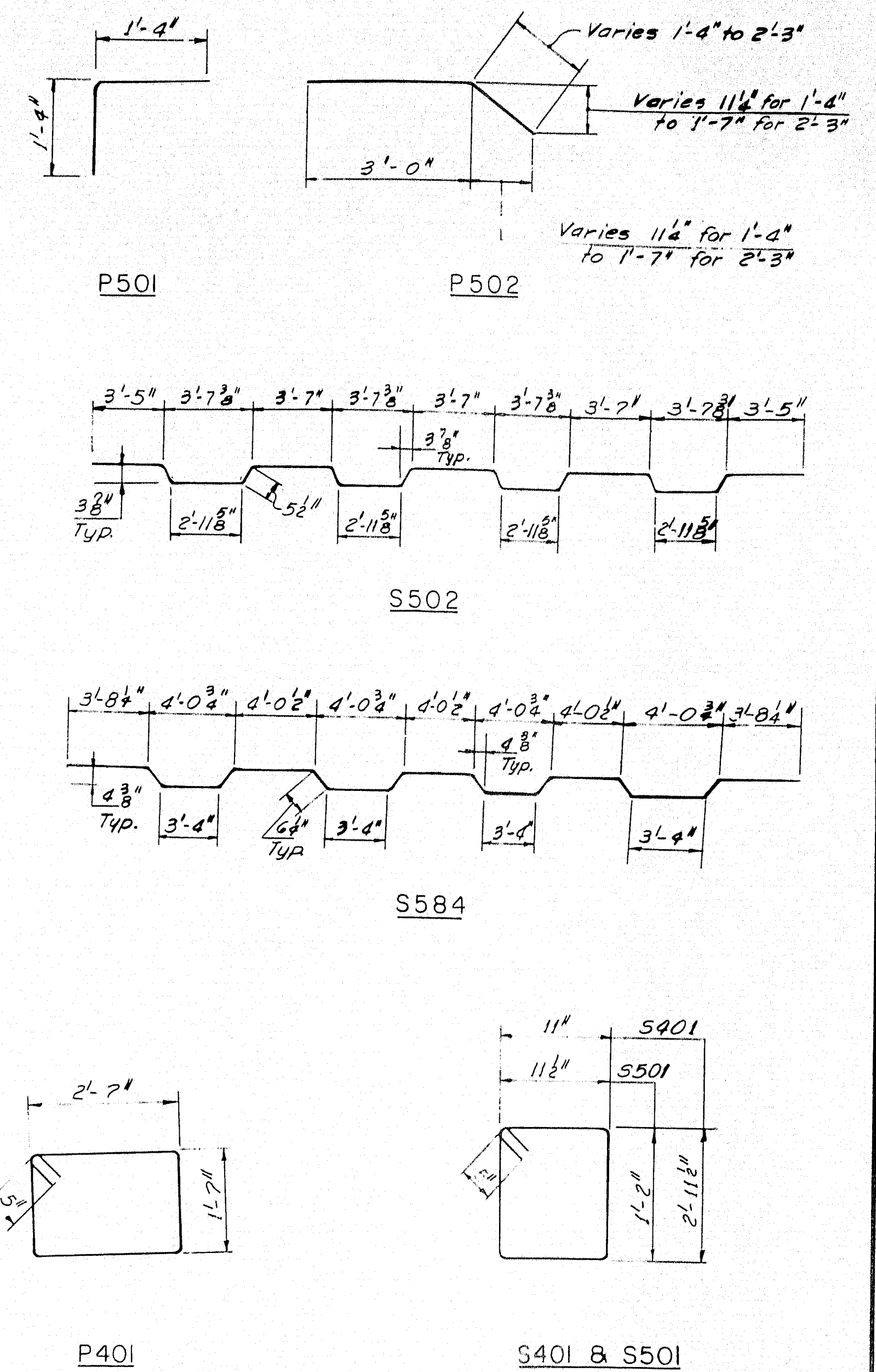
M-2269 DYE BROOK OAKFIELD(12)



PIER 2 (Southbound)				
MARK	SIZE	NUMBER	LENGTH	INCR. LOCATION
STRAIGHT BARS				
P503	5	32	26'-9"	Stem Horizontal
P505	5	32	22'-2"	Stem Horizontal
P601	6	8	30'-0"	Cap
P603	6	8	21'-11"	Cap
P604	6	112	17'-8"	Stem Vertical
P605	6	112	4'-9"	Footing Dowels
P606	6	10	30'-0"	Footing Longitudinal
P608	6	10	27'-9"	Footing Longitudinal
P701	7	58	8'-6"	Footing Transverse
BENT BARS				
P401	4	26	9'-2"	Cap
P501	5	32	2'-8"	Nosing
P502	5	64	4'-4" to 5'-3"	Nosing
PIER 1 (Northbound)				
STRAIGHT BARS				
P506	5	32	25'-6"	Stem Horizontal
P507	5	32	21'-3"	Stem Horizontal
P601	6	8	30'-0"	Cap
P605	6	108	4'-9"	Footing Dowels
P606	6	10	30'-0"	Footing Longitudinal
P609	6	108	17'-3"	Stem Vertical
P610	6	10	28'-9"	Footing Longitudinal
P611	6	8	22'-11"	Cap
P701	7	59	8'-6"	Footing Transverse
BENT BARS				
P401	4	27	9'-2"	Cap
P501	5	32	2'-8"	Nosing
P502	5	64	4'-4" to 5'-3"	Nosing
PIER 2 (Northbound)				
STRAIGHT BARS				
P506	5	32	25'-6"	Stem Horizontal
P508	5	32	18'-5"	Stem Horizontal
P601	6	8	30'-0"	Cap
P605	6	102	4'-9"	Footing Dowels
P606	6	10	30'-0"	Footing Longitudinal
P608	6	10	27'-9"	Footing Longitudinal
P609	6	102	17'-3"	Stem Vertical
P610	6	8	19'-11"	Cap
P701	7	58	8'-6"	Footing Transverse
BENT BARS				
P401	4	25	9'-2"	Cap
P501	5	32	2'-8"	Nosing
P502	5	64	4'-4" to 5'-3"	Nosing

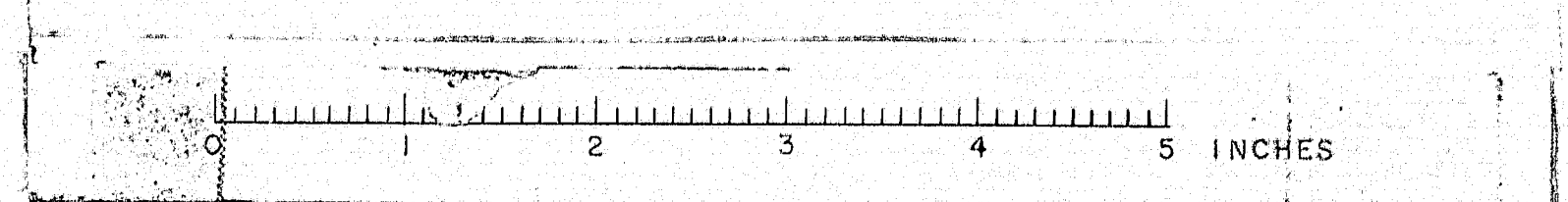
SUPERSTRUCTURE (Southbound)				
MARK	SIZE	NUMBER	LENGTH	INCR. LOCATION
STRAIGHT BARS				
S402	4	40	1'-8"	End Post
S503	5	59	35'-2"	Deck Transverse
S504	5	59	31'-0"	"
S505	5	63	20'-2"	"
S506	5	63	23'-9"	Deck Transverse
Bars S507 to S518 Not used				
S519	5	27	23'-0"	Deck Transverse
S520	5	27	23'-7"	"
S521	5	13	20'-5"	"
S522	5	13	24'-1"	"
S523	5	14	34'-11"	"
S524	5	14	31'-4"	"
S525	5	23	21'-3"	"
S526	5	23	21'-10"	"
S527	5	11	19'-11"	"
S528	5	11	22'-4"	"
S529	5	12	33'-2"	"
S530	5	12	29'-7"	"
S531	5	27	19'-9"	"
S532	5	25	20'-4"	"
S533	5	13	17'-10"	"
S534	5	13	20'-10"	"
S535	5	13	31'-8"	"
S536	5	13	28'-1"	Deck Transverse
S537	5	98	17'-8"	Deck Longitudinal
S538	5	122	20'-4"	"
S539	5	240	20'-8"	"
S540	5	120	23'-8"	"
S541	5	120	22'-10"	"
S542	5	8	8'-8"	Deck Longitudinal
S543	5	24	11'-8"	Safety Walk
S544	5	8	11'-0"	Safety Walk
S601	6	23	25'-10"	Deck Transverse
S602	6	23	26'-5"	"
S603	6	11	22'-10"	"
S604	6	11	26'-11"	"
S605	6	12	37'-9"	"
S606	6	12	34'-2"	"
S607	6	23	24'-5"	"
S608	6	23	25'-0"	"
S609	6	11	21'-8"	"
S610	6	11	25'-6"	"
S611	6	12	36'-4"	"
S612	6	12	32'-9"	Deck Transverse
BENT BARS				
S401	4	16	8'-7"	End Post
S501	5	236	5'-1"	Safety Walk
S502	5	121	33'-1 1/2"	Deck Transverse
SUPERSTRUCTURE (Northbound)				
STRAIGHT BARS				
S402	4	40	1'-8"	End Post
S544	5	62	30'-7"	Deck Transverse
S545	5	62	34'-8"	"
S546	5	61	28'-6"	"
S547	5	61	18'-5"	Deck Transverse
Bars S548 to S559 Not used				
S560	5	24	16'-11"	Deck Transverse
S561	5	24	17'-6"	"
S562	5	12	22'-1"	"
S563	5	12	18'-0"	"
S564	5	12	30'-2"	"
S565	5	12	34'-2"	"
S566	5	24	15'-8"	"
S567	5	24	16'-3"	"
S568	5	12	20'-10"	Deck Transverse

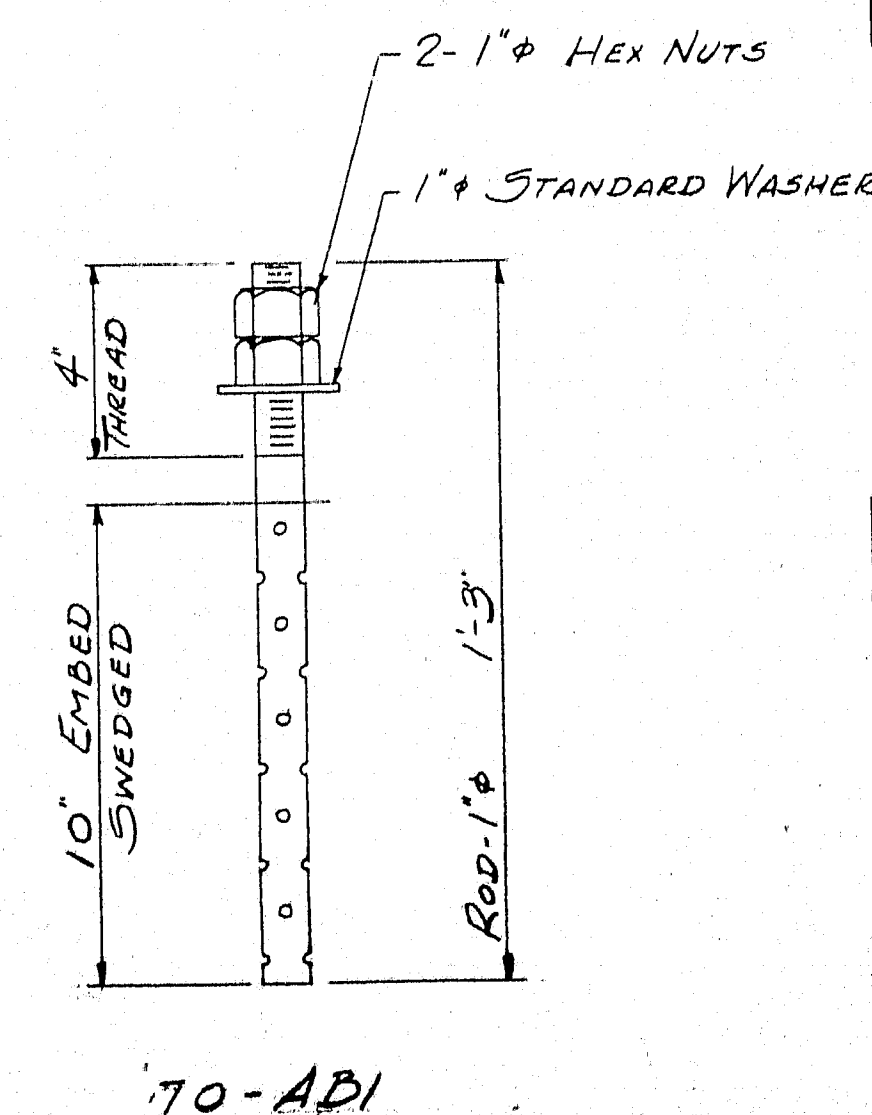
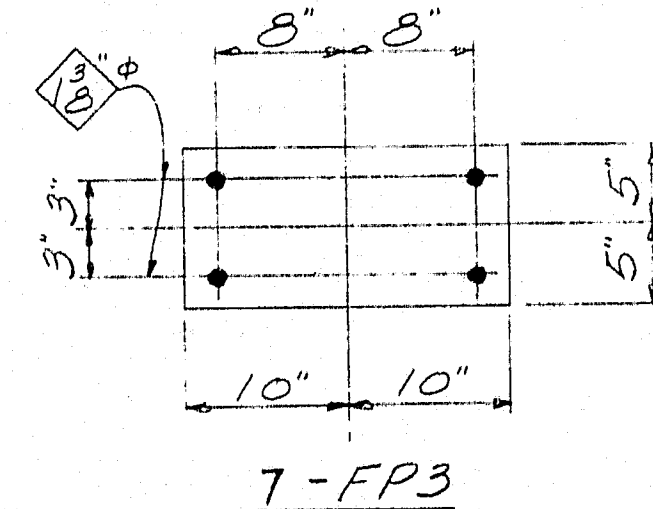
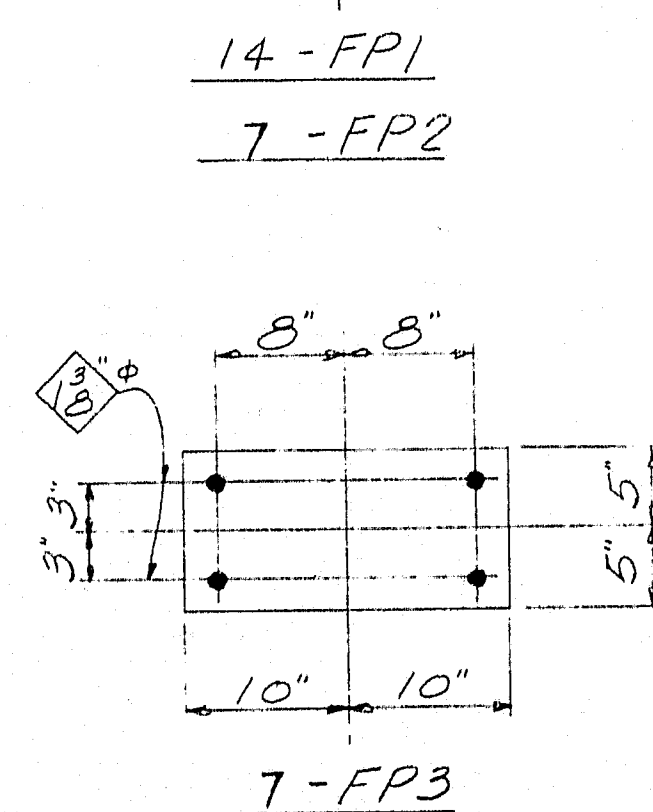
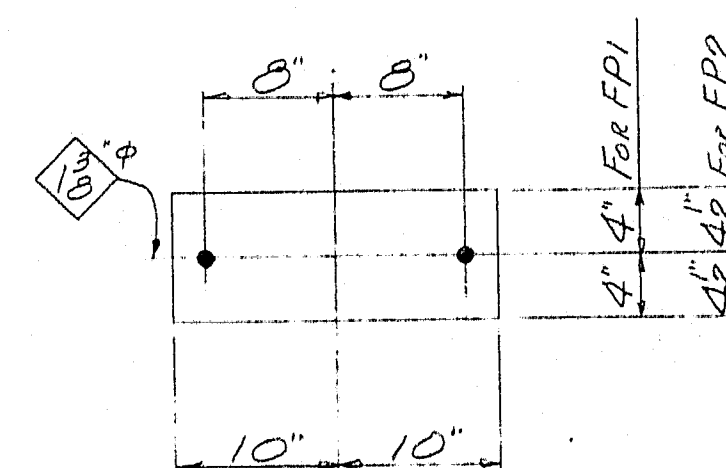
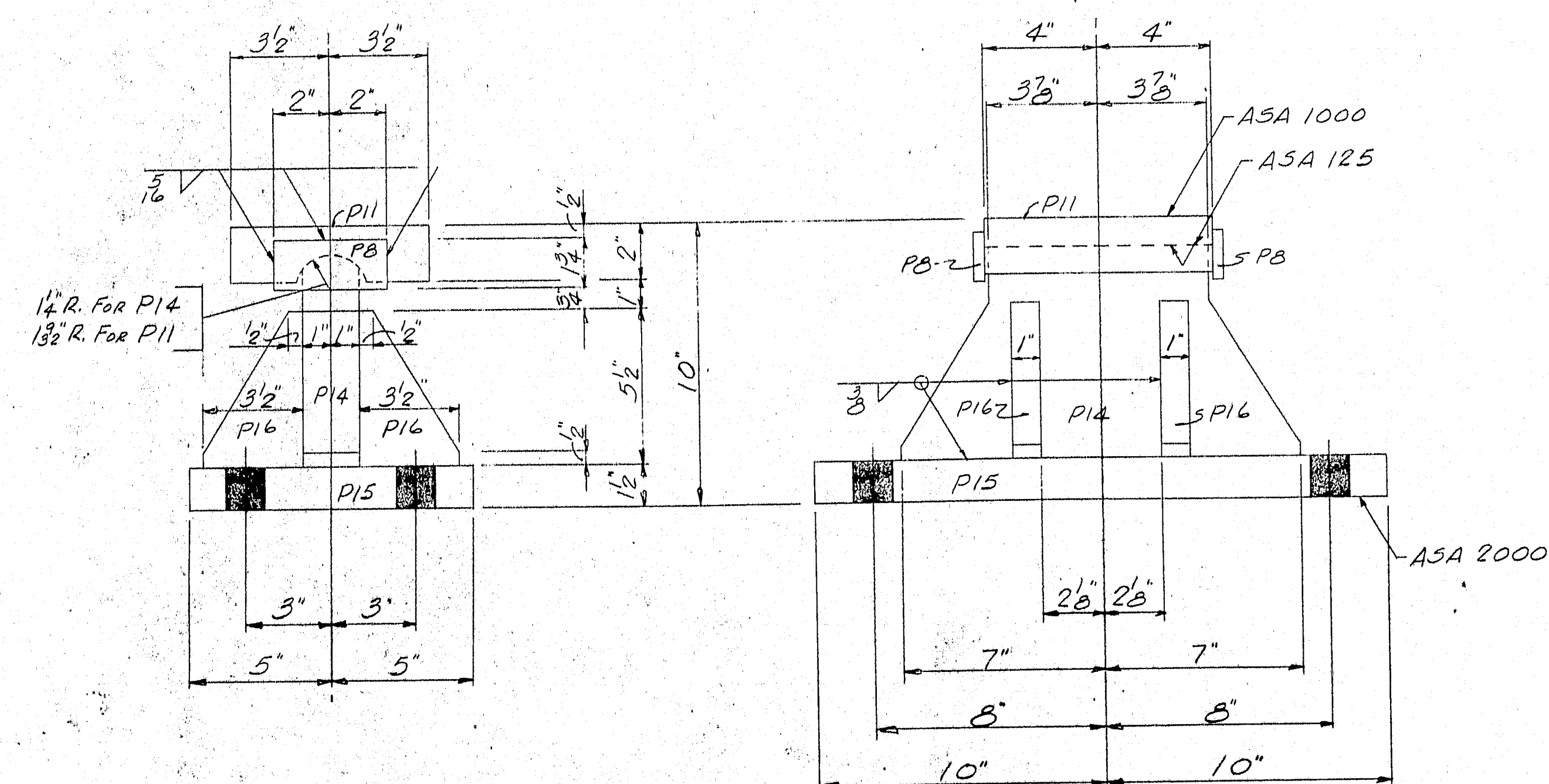
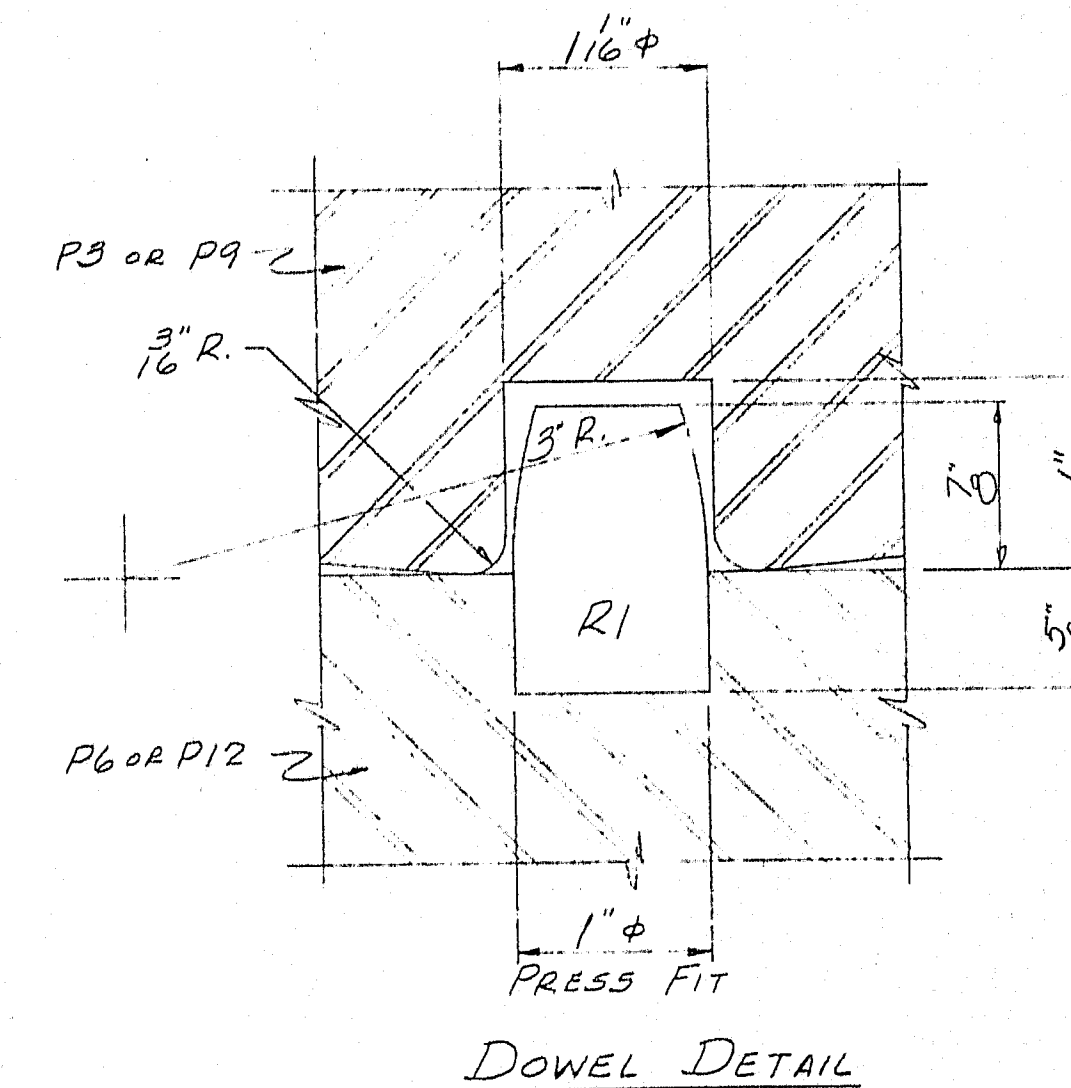
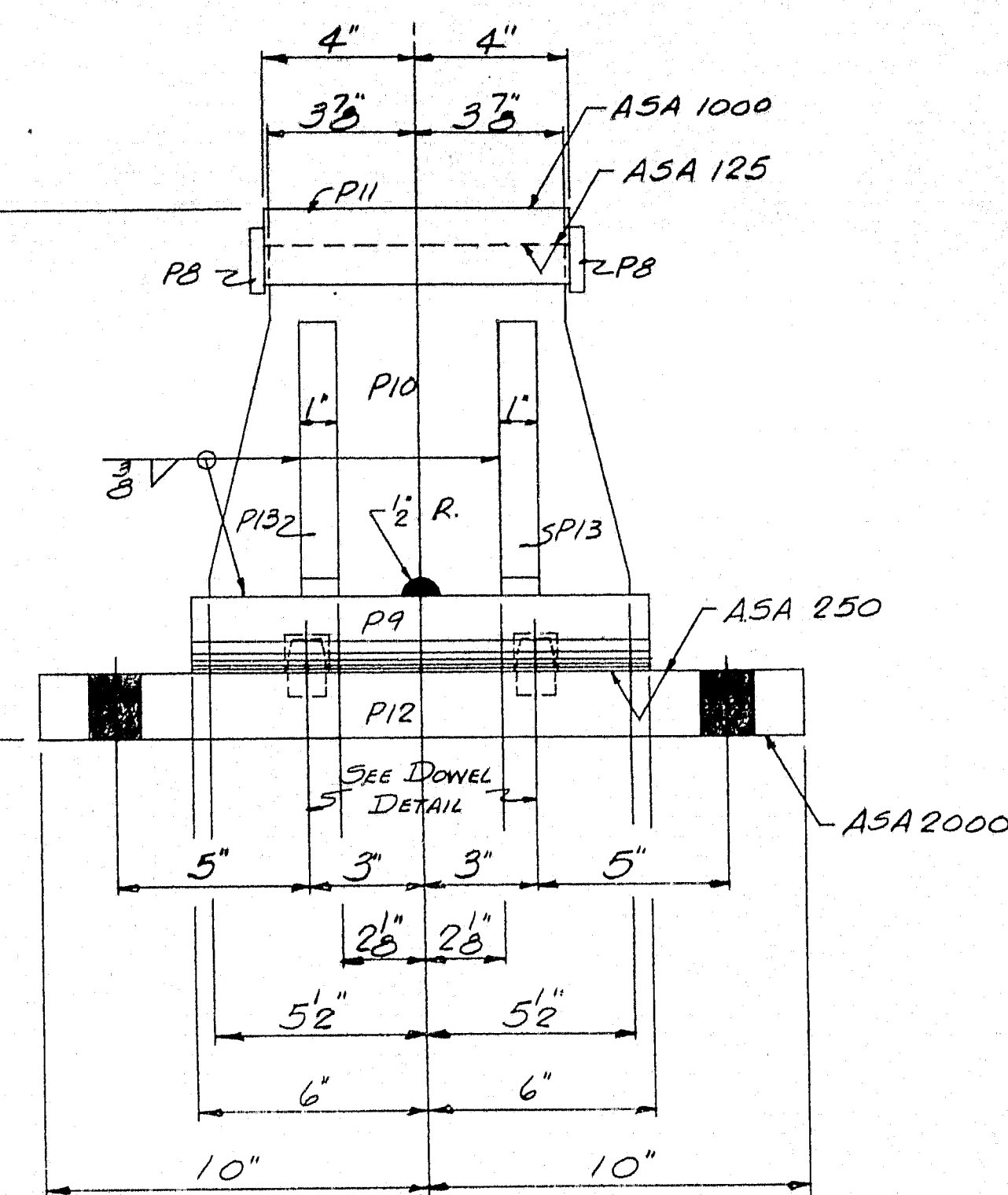
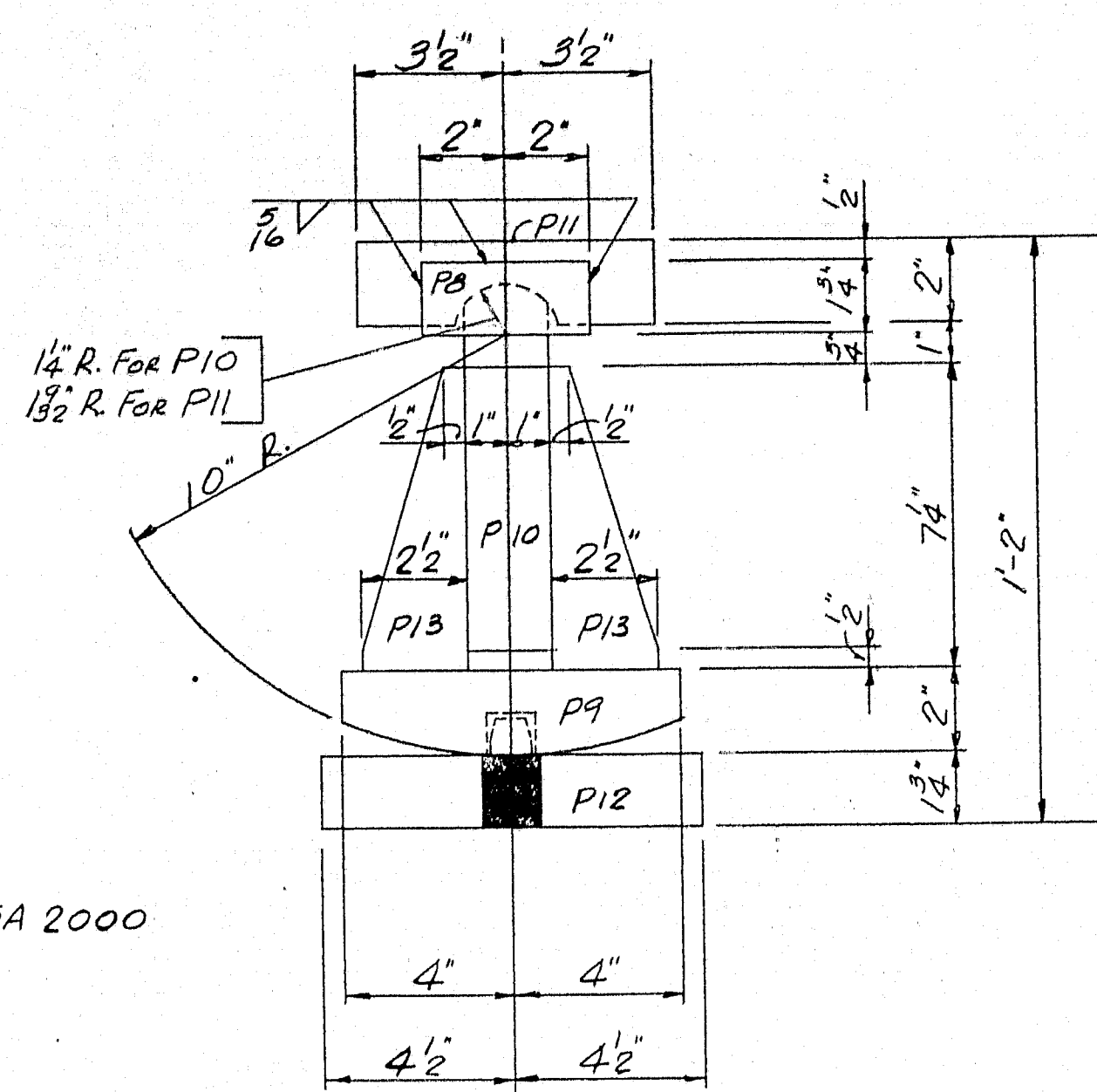
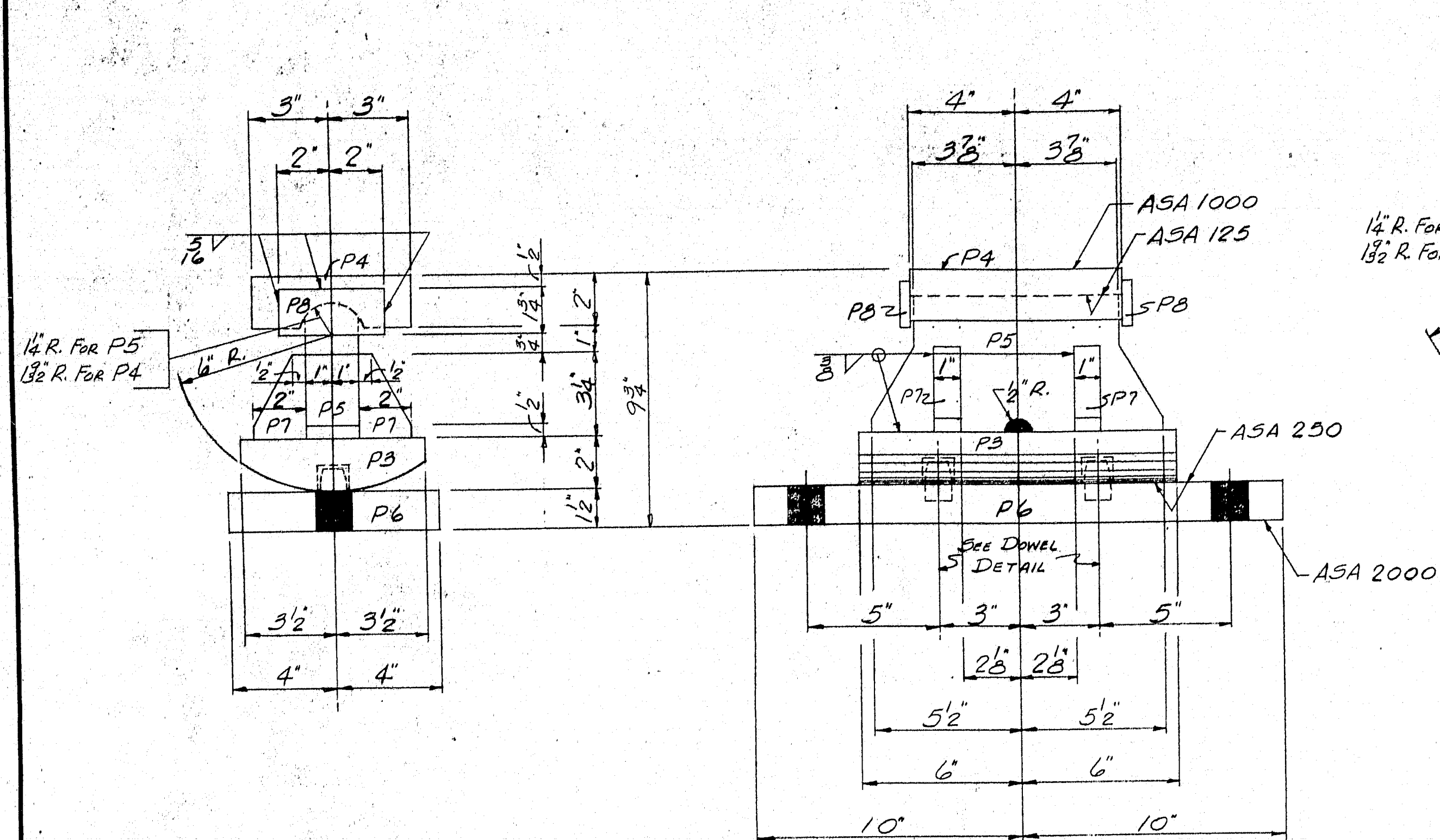
SUPERSTRUCTURE (Northbound) Continued				
MARK	SIZE	NUMBER	LENGTH	INCR. LOCATION
STRAIGHT BARS				
S569	5	12	16'-9"	Deck Transverse
S570	5	12	28'-11"	"
S571	5	12	32'-11"	"
S572	5	26	14'-5"	"
S573	5	26	15'-0"	"
S574	5	13	19'-7"	"
S575	5	13	15'-6"	"
S576	5	13	27'-8"	"
S577	5	13	30'-8"	Deck Transverse
S578	5	116	17'-6"	Deck Longitudinal
S579	5	123	21'-4"	"
S580	5	369	21'-8"	"
S581	5	123	23'-10"	Deck Longitudinal
S582	5	8	8'-8"	Safety Walk
S583	5	8	12'-8"	"
S585	5	16	10'-6"	"
S586	5	8	11'-9"	Safety Walk
S641	6	24	19'-5"	Deck Transverse
S642	6	24	20'-0"	"
S643	6	13	24'-7"	"
S644	6	13	20'-6"	"
S645	6	12	32'-8"	"
S646	6	12	36'-8"	"
S647	6	24	18'-2"	"
S648	6	24	18'-9"	"
S649	6	12	23'-4"	"
S650	6	12	19'-3"	"
S651	6	12	31'-5"	"
S652	6	12	35'-5"	Deck Transverse
BENT BARS				
S401	4	16	8'-7"	End Post
S501	5	244	5'-1"	Safety Walk
S584	5	122	37'-0"	Deck Transverse



- NOTES:
1. All dimensions are to c. of bars.
 2. All reinforcing bars shall be intermediate grade steel.
 3. Reinforcing steel to have 2" minimum cover unless otherwise shown.

DESIGN - G.H.	DETAIL - J.R.A.	BRIDGE NO.
TRACE - P.R.N.		SURVEY - PLOT
STATE HIGHWAY COMMISSION BRIDGE DIVISION		
INTERSTATE 95 OVER RELOCATED EAST BRANCH MATTAWAMKEAG RIVER IN THE TOWN OF OAKFIELD ARROSTOOK COUNTY REINFORCING STEEL		
HOWARD, NEEDLES, TAMMEN & BERGENDOFF CONSULTING ENGINEERS		SHEET 16 OF 16
NEW YORK	BOSTON	AUGUSTA, MAINE
		FEBRUARY 1965
M-2270 DYER BROOK OAKFIELD (12)		





PAINT NOTES

NO PAINT ON ANCHOR BOLTS - OIL THREADS. NO PAINT
ON TOP SURFACE & 1" DOWN FROM TOP ON SIDES OF SOLE
PLATES. COAT WITH BOILED LINED OIL. ~~DO NOT~~
~~COAT SURFACES EXPOSED TO SEA WATER.~~ DO NOT COAT SURFACE
~~EXPOSED TO SEA WATER.~~ ONE SHOP COAT PAINT ON SURFACES
FINISHED ASA 250. NO PAINT ON SURFACES ASA 125,
COAT WITH HOT MIXTURE OF WHITE LEAD & TALLOW.

SHIP		BILL OF MATERIAL			DWG. <u>DC-115-92</u>	
MARK	NO.	MARK	SHAPE	LENGTH	WT.	REMARKS
EPC1	14		PEDESTAL			
	14	P3	R-7x2	1	0	A36
	14	P4	R-6x2	0	8	
	14	P5	R-5x2	0	11	
	14	P6	R-3x1 1/2	1	8	
	56	P7	R-2x1	0	34	
	28	P8	R-1 3/4 x 3/8	0	4	
EPC3	7		PEDESTAL			
	7	P9	R-8x2	1	0	A36
	7	P10	R-9x2	0	11	
	7	P11	R-7x2	0	8	
	7	P12	R-9x1 3/4	1	8	
	28	P13	R-2 1/2 x 1	0	7 1/2	
	14	P8	R-1 3/4 x 3/8	0	4	
FPC2	7		PEDESTAL			
	7	P14	R-7 1/2 x 2	1	2	
	7	P11	R-7x2	0	8	
	7	P15	R-10x1 1/2	1	8	
	28	P16	R-3 1/2 x 1	0	5 1/2	
	14	P8	R-1 3/4 x 3/8	0	4	
	42	R1	ROD-1" φ	0	1 1/2	A36
FP1	14		3 x 3	1	8	FABCO PAD "SA47
FP2	7		9 x 3	1	8	Do Do
FP3	7		10 x 3	1	8	Do Do
AB1	70		ROD-1" φ	1	3	THREADED & SWEDGED } A36
	140		1" HEX NUT			REQ. # 3881
	70		1" STD. WASHER			

WELD WITH LH-E7028 OR
LH-E6028 OR SAW-14 PREHEAT
SHOP CONNECTIONS: 1" TO 2" THK. MAT. TO 50" F.
FIELD CONNECTIONS: BOLT & WELD
HOLES: 1 3/8" & U.N.
PAINT: STATE OF MAINE SPEC'S & SEE
PAINT NOTE THIS SHEET
65 PROJ. NO. I-95-9 (12)

APP'D. AS NOTED 8-10-6
APP'D. 7-29-65

2 2

BEARING PEDESTALS & ANCHOR BOLTS NORTHBOUND

PRINT ISSUE

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5	S.H.C.	9-10-65
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3	Cust.	9-19-65
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4	PORT.	9-8-6
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2	F.A.	8-6-65
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4	PORT.	7-28-65
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2	F.A.	7-28-65
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DRAWN	7-28-65	R.A.
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REVISION	8-4-65 R.A.
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REVISION		

Bancroft & Martin Inc.
Brewer, Maine

5 I-95 OVER E. BRANCH MATTAWAMKEAS RIVER
5 OAKFIELD, MAINE

CUSTOMER CIANCHETTE BROS., INC.
DESIGNER M.S.H.C., BRIDGE DIVISION

ORDER	VERBAL	DWG. B65-135-52
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97-179

